



DOWNTOWN ELSINORE

SPECIFIC PLAN

LAKE ELSINORE, CA



DRAFT
SEPTEMBER 4, 2018





DOWNTOWN ELSINORE SPECIFIC PLAN

Lake Elsinore, CA

PREPARED FOR:

THE CITY OF LAKE ELSINORE
130 SOUTH MAIN STREET
LAKE ELSINORE, CA 92530

PREPARED BY:

CITY OF LAKE ELSINORE COMMUNITY DEVELOPMENT DEPARTMENT
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ADOPTED BY CITY COUNCIL: _____, 2018
ORDINANCE NO. 2018-_____



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1 INTRODUCTION

1.1 PURPOSE OF THIS DOCUMENT

A specific plan is a tool for the systematic implementation of a general plan. It establishes a link between a general plan's policies and individual development proposals in a defined area. This link is established through detailed planning policies that refine the general plan policies applicable to a defined area. Thus, a specific plan designates land uses together with detailed policies and regulations into a focused development scheme.

The Downtown Elsinore Specific Plan (Specific Plan) provides a vision and strategic framework to guide future development in the City's historic downtown. It capitalizes on the City's unique assets with the overarching goal of vitalizing downtown and implementing the City's vision that "The City of Lake Elsinore will be the ultimate destination where all can live, work, and play, build futures and fulfill dreams." The Specific Plan will draw residents and visitors to the City's historic Main Street corridor by encouraging a mixed-use downtown area that has a variety of commercial and residential uses, including restaurants with outdoor dining, entertainment, hotel, office, retail, service, high density and affordable housing, cultural and civic uses.

1.1.1 2011 Downtown Master Plan

On December 13, 2011, the City of Lake Elsinore's City Council adopted the Lake Elsinore Downtown Master Plan as a vision document for the downtown area, themed "Connecting ... to the Lake." Indeed, the City's primary asset is Lake Elsinore, the largest natural lake in Southern California, a true recreational paradise for boating, parasailing, fishing, jet skiing, picnicking, or camping at local campgrounds and RV resorts. The 2011 Master Plan sought to preserve historic assets within the downtown while also allowing the downtown to evolve as a "City of its Time." The 2011 Master Plan emphasized opportunities to create place as an outdoor recreation resort environment centered around a downtown city on a lake. The 2011 Master Plan looked to the City's authentic buildings and heritage, a thriving cultural diversity, and attractive natural resources in which to spawn a lively place to invest, live, work, educate and play in Downtown Lake Elsinore.

During the development of the 2011 Downtown Master Plan, the City's public outreach engaged the community through open dialogue, consensus-building workshops and

citizen surveys. Public input identified what would make downtown a viable and livable area, such as a quality public realm, and Main Street as a great street.

In the intervening seven years since the Downtown Master Plan was adopted, the City has seen significant economic growth. Development interest in the downtown area, from restaurateurs and resort destination developers, has been encouraging. However, the existing 2011 Master Plan has been confusing to implement. It includes five (5) diverse “districts,” each with a limited land use plan intended to promote separate communities, and consists of four separate documents (the Downtown Master Plan vision, the Key to Downtown Implementation Plan, the Downtown Code, and the Economic and Feasibility Analysis). For these reasons, it has been difficult to accommodate potential development in the downtown area and the 2011 Master Plan is no longer in step with current market demands.

1.1.2 Downtown Elsinore Specific Plan

The Downtown Elsinore Specific Plan represents a reimagining of the 2011 Master Plan. The Downtown Elsinore Specific Plan is intended to:

- Collate the existing four separate documents into one simplified specific plan document.
- Eliminate the five separate “districts.” Planning goals will identify and guide land uses that optimize their location within the downtown framework.
- Provide a simplified, user-friendly Land Use Matrix.
- Provide flexible signage options.
- Provide flexible architectural principles and guidelines.
- Eliminate the Main Street Circle (roundabout) concept.
- Eliminate the Garden District concept and replace the Main Street area with a Mixed Use designation.
- Refine the Plan’s boundary area and locate “Gateway to the Downtown” monumentation at the intersection of Flint and Main Streets.

The Downtown Elsinore Specific Plan removes property between Spring Street and Ellis Street, and between Flint Street and the Interstate 15 Freeway, from the plan area in order to recognize existing industrial and commercial land uses that are incompatible with the Downtown Elsinore Specific Plan. This area will revert to Business Professional and

General Commercial land use designations in the General Plan. Zoning will revert to M-1 (Limited Manufacturing) and C-2 (General Commercial), respectively. Consistent with this change, “Gateway to the Downtown” entry monuments, signage, and landscaping will begin at the intersection of Flint Street and Main Street.

1.2 REGIONAL AND LOCAL SETTING

The City of Lake Elsinore is located in the southwest portion of Riverside County, between the cities of Corona to the north and Wildomar and Murrieta to the south. The metropolitan areas of Los Angeles and San Diego are within 100 miles to the northwest and south, respectively. (Figure 1-1)

The City of Lake Elsinore is served by major freeways connecting it to Orange County, central Riverside County, San Diego County and San Bernardino County. Major north-south access is provided by Interstate 15 (I-15), which connects the community to San Diego County to the south, and central Riverside County and San Bernardino County to the north. Additionally, State Route 74 (SR-74) provides connection to the City of Perris to the east and south Orange County to the southwest.

1.3 DOWNTOWN PLAN BOUNDARY

The 178-acre Downtown Elsinore Specific Plan covers an approximately mile-long area extending from Flint Street along its northern edge, southwesterly to the edge of the Lake. It is generally bounded by Riley Street and the Lake Elsinore Outlet Channel on the west and Ellis Street and Chestnut Street on the east. (Figure 1-2) Included in the Plan area is the historic downtown, located on Main Street. Main Street extends through the Specific Plan from Flint Street to Lakeshore Drive along the historic route US 395.

1.4 DOWNTOWN VISION

The Downtown Elsinore Specific Plan represents the community’s Vision for the future of Downtown Lake Elsinore. This Vision was initially developed during the preparation of the 2011 Master Plan, when the City made a concerted effort to involve and listen to the citizens of Lake Elsinore. Citizens expressed a consistent desire to have a livable, viable and memorable place for residents and visitors alike. The Downtown Elsinore Specific Plan incorporates and expands this Vision that affirms the downtown as the heart of the City.

The Downtown Elsinore Specific Plan has built-in flexibility allowing growth to occur naturally over time. The plan offers the ability for the market to drive the timing of private development, while the public sector has the ability to seed public development such that the entire plan is not subject to waiting for the market, and the market is not forced to under build but will be able to build to the market and beyond when conditions are right.

The following concepts are the key components of the Downtown Elsinore Specific Plan's Vision:

Vision: Enhance Downtown vitality by creating a safe, vibrant and walkable downtown centered along Main Street.

A common feature of successful downtowns is an attractive, walkable streetscape with a mix of shopping opportunities, restaurants, and housing in both well maintained historic buildings and high quality new development and open spaces. The preservation of the existing historic core of downtown and the creation of further opportunities for infill and redevelopment along the Main Street corridor will become memorable by the way in which civic buildings and public spaces are positioned to create a sequence of events and experiences from both a vehicular and a pedestrian point of view. The sequence of events critically sets the stage for the ultimate termination and celebration of the lake at the southern end of Main Street. This reestablishes the lake as the most valuable resource within downtown, both from an environmental perspective and as an economic generator.

Vision: Make Downtown a centralized destination for community activities and for 18-hour a day activities for families and individuals.

The Downtown is the traditional home of many family-friendly community activities including Winterfest, Car Cruises, Dia de los Muertos and the Lake Elsinore Grand Prix. The Downtown Elsinore Specific Plan envisions the Downtown as a restaurant, entertainment, cultural, artistic, educational, and civic center for the City that will become a vibrant, "18-hour" location where people will drive less, walk more and socialize after work.

Vision: Create a "park-once-and-walk" district.

The intent of the Downtown Elsinore Specific Plan is to create an overall parking concept that results in "just enough" public and private parking to serve the needs of people

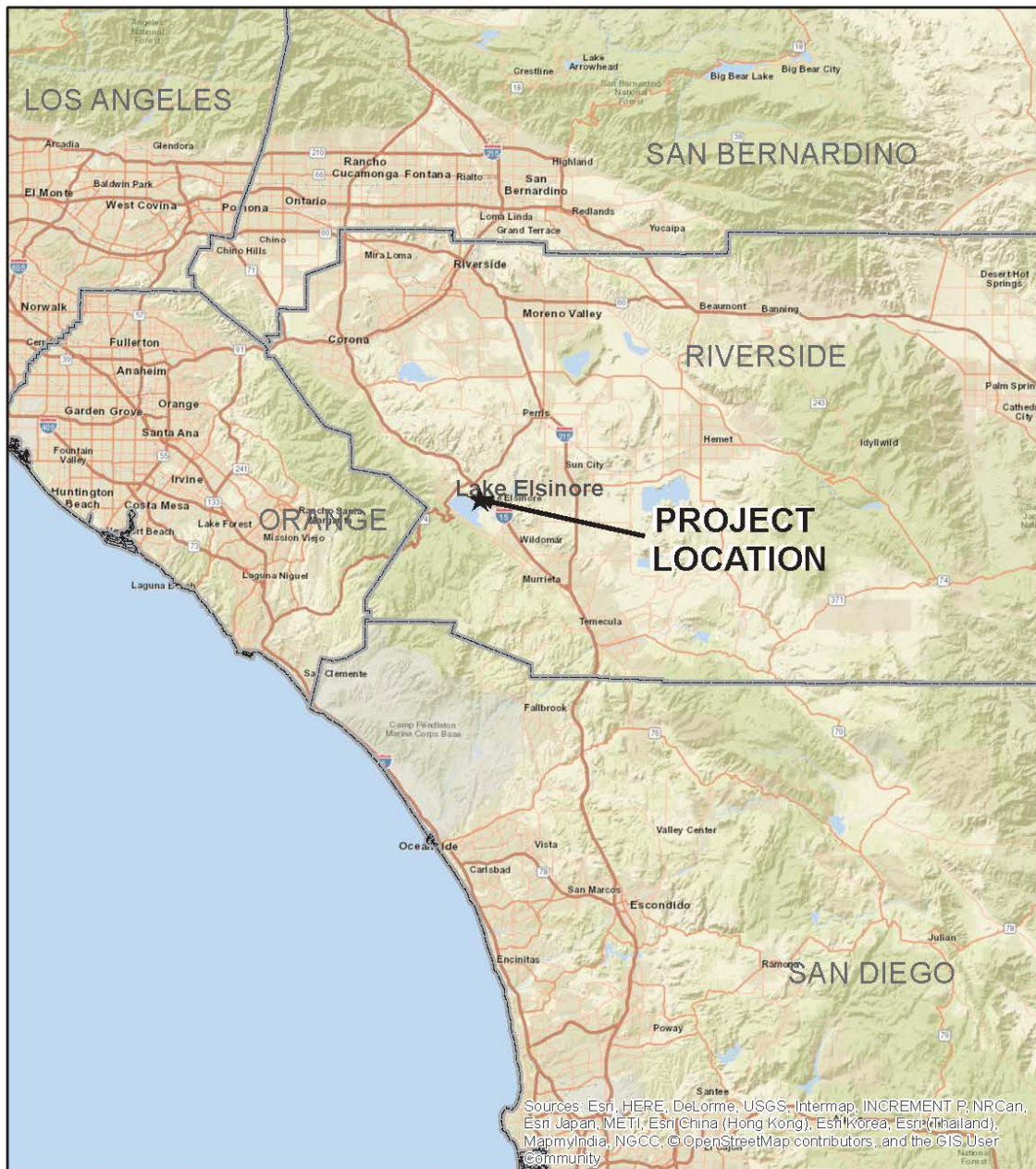
living, working and visiting Downtown Lake Elsinore. Parking will be located in a manner that will create a “park-once-and-walk” district.

The benefit of a walkable downtown is that it allows people to park once and walk to shops and restaurants. People will be able to take their time, window shop, go in and out of stores, socialize and exercise. The provision of adequate sidewalk widths, good lighting and signage, interesting building facades and shop windows make for an enjoyable downtown experience. A parking supply that is adequate to serve a downtown, but does not overwhelm it with lots of pavement, will support accessibility to downtown.

1.5 DOWNTOWN ELSINORE SPECIFIC PLAN OBJECTIVES

The following objectives of the Downtown Elsinore Specific Plan provide a framework for purpose and intent of the Plan.

1. Set forth a flexible development plan that implements the City of Lake Elsinore General Plan and achieves the City’s development goals for the historic downtown area.
2. Stimulate private sector investment in the downtown area.
3. Enhance connectivity from the downtown to the lake with the realignment of Main Street, Lakeshore Drive and Library Street.
4. Create a vibrant, livable and sustainable environment in downtown.
5. Encourage the infill of vacant lots, especially corner lots, with quality mixed-use development with a variety of residential, office/commercial, retail, cultural and civic uses.
6. Promote the inclusion of public buildings, such as a library, post office, and city hall, as economic magnets in the downtown.
7. Provide for a variety of cultural and arts events that will draw residents and visitors to the downtown.
8. Protect the character of historic buildings within the historic downtown.
9. Create walkable streets, with new sidewalks, streetscaping, and quality lighting.
10. Provide adequate parking.



**FIGURE 1-1
REGIONAL MAP**

Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

1.6 AUTHORITY FOR PLAN

The Downtown Elsinore Specific Plan has been prepared under the authority granted to the City of Lake Elsinore in accordance with the requirements of California Government Code, Title 7, Division 1, Article 8, Sections 65450 through 65457.

Section 65450 states that “[a]fter the legislative body has adopted a general plan, the planning agency may, or if so directed by the legislative body, shall, prepare specific plans for the systematic implementation of the general plan for all or part of the area covered by the general plan.”

Section 65451 mandates that a specific plan contain the following:

“(a) A specific plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
- (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- (4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).

“(b) The specific plan shall include a statement of the relationship of the specific plan to the General Plan.”

Section 65453 also allows specific plans to be adopted either by resolution or by ordinance and to be amended as often as deemed necessary by the legislative body.

2 CONTEXT

2.1 HISTORIC CONTEXT

HISTORY OF LAKE ELSINORE

The earliest known inhabitants in the area were the Luiseño Indians. The village of Páayaxchi was located northwest of the Lake and this name also refers to the Lake itself. The area around and including the Elsinore hot springs was known to the Luiseño as iténgvu Wumówmu (meaning “hot springs”). The hot springs also figure prominently into Luiseño oral tradition. The location, iténgvu Wumówmu, is named in a song about the death of Wuyóot, a religious leader who led the people in their migration from the north. Several additional Luiseño place names are within the Lake Elsinore area including We'éeva, Pí'iv, Qawiimay, Páayaxchi Nivé'wuna, and Anóomay.

SETTLEMENT

The Spaniards called the lake “Laguna Grande”. The lake was later part of the Mexican land grant, “Rancho La Laguna,” which was given by Governor Manuel Micheltorena to Julian Manriquez in 1844. His adobe ranch house lay at the north end of the west side of the lake. He sold the land to Abel Stearns in 1851. Stearns subsequently sold the 13,339-acre rancho to Augustin Machado in 1858. Machado built his house and outbuildings near the southwest corner of the lake. From 1857 to 1861 the rancho was a stop on the Butterfield Overland Mail between Los Angeles and Fort Yuma. The old Manriquez adobe was used as the site of the stage station.

In 1873, Augustin Machado's son, Juan, sold all but 500 acres of the rancho to Charles Sumner, an English settler. Juan Machado retained the 500 acres on the west corner of the lake where his house still stands near Grand Avenue.

FOUNDING FATHERS

In 1883, the rancho lands held by Sumner were purchased by City founder Franklin Heald in partnership with Donald Graham and William Collier for \$24,000, realizing a dream that will endure and evolve as the future City of Lake Elsinore. City founders included Franklin Heald, Donald Graham, William Collier and Margaret Collier Graham (brother to William and wife to Donald). Margaret Collier Graham picked the name “Elsinore” because she liked the sound, a name taken from Shakespeare's Hamlet.

CITY INCORPORATION

The residents voted to incorporate the City on April 9, 1888. At that time, Elsinore was in San Diego County. In 1893, the City became part of the newly formed County of Riverside.



BOOM TOWN

In 1888, the City population had approximately 1,000 residents, and boasted a variety of stores, a lumberyard, daily mail service, brick buildings downtown, and good water, soil and climate for agriculture.



HEALING WATERS

As a prominent stop along the Butterfield Stage Route, the word of the healing mineral and sulphur water within the City quickly spreads. From the beginning, the City's mineral springs have attracted visitors seeking therapeutic treatments from the late 19th Century. Built in 1887, The Crescent Bath House, also known as The Chimes, was one of the first buildings in the downtown. The building still stands today and is a registered national historic site.



INDUSTRIES

In 1888, the industries that supported the economy included coal and clay mining, gold mining, ranching and agriculture (fruit, nuts). The Good Hope Mine, located in the hills between Elsinore and Perris yielded two million dollars' worth of gold during its' working years and is operated on and off for 90 years before high groundwater ended its run and forced its' closure. The railroad first served the town in 1885. The Santa Fe train depot still stands

today, at the corner of Graham Avenue and Spring Street, which serves as the Lake Elsinore Valley Chamber of Commerce headquarters



A GOLDEN AGE

The 1920s Lake Elsinore was a playground for the rich and glamorous, from the Hollywood and Los Angeles. The Valley hosted Olympic teams for training and high speed boat racing on the lake.

DEVELOPMENTS IN THE CITY

In 1964, Elsinore elected California's first African-American Mayor, Thomas R. Yarborough. He was elected to the City Council in 1948. In the 1950s, the lake completely dried up for the first time in recorded history, which lasted for a period of 10 years. In 1964, the lake was artificially filled with water from the Colorado River, delivered via Metropolitan Water District's Colorado River Aqueduct. The Skylark Airport, located in the floodplain at the southern end of the lake, emerged as a world class sky diving drop zone.



Historic Main Street and City Hall were completely renovated in 1989. The Lake Elsinore Outlet Center opened in 1991, boasting over 100 factory outlets. Professional baseball came to town with the opening of the Lake Elsinore Diamond Baseball stadium in 1994, the home of the Lake Elsinore Storm, first an affiliate of the

Anaheim Angels, later and currently with the San Diego Padres.

IT'S THE NAME: THE CITY OF LAKE ELSINORE

In 1972, the citizens by popular vote, approved a new name: the City of Lake Elsinore. This name change was favored as a way to promote the City as a lake-oriented destination area. The State of California Department of Parks and Recreation owned the lake and a campground recreation area until 1993, when those assets were conveyed to the City.



2.2 HISTORIC STRUCTURES

Several historical structures are located within, and in proximity to the Specific Plan area. These structures provide context and reference for the Downtown Elsinore Specific Plan's architectural and cultural character. (See Table 2-1 and Figure 2-1) The degree to which historic structures influence the design direction within the Downtown Elsinore Specific Plan may vary; however, protection of existing noteworthy structures and respect for the City's heritage is a theme that will guide new development.

Two historic buildings within the boundary of the Downtown Elsinore Specific Plan are listed in the National Register of Historic Places: the Crescent Bath House/Chimes Building and the Grand Army of the Republic Armory Hall. The Chimes Building, built in the 1800's was one of the first buildings to be built within the City of Lake Elsinore. The list of California Points of Historical Interest includes the Grand Army of the Republic Armory Hall and the Lake Elsinore Hot Sulphur Springs.

In the 1980's, the Riverside County Historical Commission designated the downtown as a local historic district. The historic downtown is specifically focused on Main Street, Heald Avenue, and Graham Avenue.

Table 2-1 Historic Structures within Downtown Elsinore Specific Plan and Vicinity

SITE TYPES	SITE DEFINITIONS AND LOCATIONS
Listed – National Register of Historic Places	Crescent Bath House/Chimes Building (P33-6998) 201 W. Graham Grand Army of the Republic Armory Hall (RIV-070) 252 South Main
Listed – California Points of Historical Interest	Grand Army of the Republic Armory Hall (RIV-070) Elsinore Women’s Club (RIV-071) 710 West Graham Elsinore’s Hottest Sulphur Springs (RIV-023) – Crescent Bath House/Chimes Building
Riverside County Historical Landmarks	Lake Elsinore Historic District (P33-7142) Elsinore’s Hottest Sulphur Springs (RIV-023) – Crescent Bath House/Chimes Building Lake Elsinore Cultural Center/Methodist Episcopal Church (183 N. Main Street) (RIV-087) Santa Fe Train Station (P33-6997) Lake Elsinore Tank House (132 W. Graham) Mary J. McDonald Fountain and Building
Community Recognized Significant Historical Resources	Chimes Building (1887) - 201 West Graham Avenue First Presbyterian Church (1889) (P33-7040) - 105 North Lindsey Street Methodist Church - Main Street at Heald Train Depot (1896) (P33-6997) - 132 West Graham Avenue Heritage Home (Strickland Home) (1905) – 700 West Graham Avenue Elsinore Women’s Club (1925) - 710 West Graham Avenue Lake Theatre (1950) (P33-7001) – 310 West Graham Avenue
Local Historic Homes of Interest	219 N. Riley Street (1920) 226 East Franklin Street (1924) (“Scotty’s Castle”) 228 S. Spring Street (1912) (“Gardner Home”) 257 Hill Street

The following describes the principal structures and their features (See Figure 2-1, Historic Structures, for locations.)

1. Grand Army of the Republic Armory Hall

This two-story structure at 252 N. Main Street is the oldest public building in Lake Elsinore. On July 4, 1887, it was dedicated by Elsinore's Chapter of the Grand Army of the Republic. As the ranks of the Civil War veterans in this organization were gradually thinned, the building became the Odd Fellows Hall, then the American Legion Hall. It was then commercialized and recently has been the home to different commercial uses. Armory Hall is listed on the National Register of Historic Places.



2. Crescent Bath House/Chimes Building

The Crescent Bath House located at 201 W. Graham Avenue was built in 1887 by Frank Ferris. The two-story bathhouse utilized the hot sulfur water, which came to the surface at a temperature of 132 degrees, and provided guests a choice of plunge, sweat, mud or porcelain tub baths. The building, is now known as the "Chimes Building". "The Chimes" was an antique shop from the 1950's until the mid-2010's. The Chimes Building is listed on the National Register of Historic Places.



3. Lake Elsinore Cultural Center/Methodist Episcopal Church

Built in 1923-24, the Cultural Center located at 183 N. Main Street was originally constructed as a Methodist Episcopal Church. Today, the building is the City of Lake Elsinore Cultural Center and serves as a public meeting hall. The Lake Elsinore Museum, operated by the Lake Elsinore Historical Society, is located in the basement. The Cultural Center is Riverside County Historical Landmark RIV-87.



4. Santa Fe Train Station

The Santa Fe Train Station was built in 1896 and moved to the present location from Arlington. Today, this depot building located at 132 W. Graham Avenue serves as the offices for the Chamber of Commerce. The Santa Fe Train Station is a Riverside County Historical Landmark.



5. Lake Elsinore Tank House

The Tank House was built in 1880 as a single-story tank house for a hot mineral spring and part of a health spa complex. This structure was remodeled in 1930 with a ground floor added and the original first floor hoisted, becoming a two-story structure. This very narrow two-story structure was moved from its original location at 116 Lakeshore Drive to its current location behind the Santa Fe Train Station at 132 W. Graham Avenue in 2011. The Lake Elsinore Tank House is a Riverside County Historical Landmark.



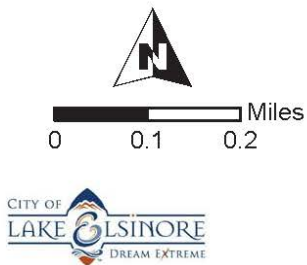
6. The Mary J. McDonald Fountain and Building

The Mary J. McDonald Fountain and Building was built in 1931 for Mary J. McDonald by Steve Coogan. Mrs. Mary J. McDonald, a key figure in the settlement and early development of the Elsinore Valley, had a fresh water well drilled at the corner of Heald and Riley Streets. Mrs. McDonald had a park built around the well site and also ordered the construction of a 25-foot diameter gazebo over the well with a fountain that provided fresh water for all Lake Elsinore citizens to drink. The fountain and seats are now gone from the gazebo. The Mary J. McDonald Fountain and Building is a Riverside County Historical Landmark.





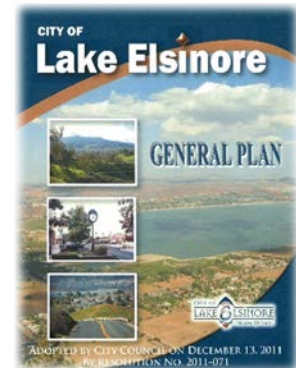
**FIGURE 2-1
HISTORIC STRUCTURES**



Prepared by:
City of Lake Elsinore GIS
August 7, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

2.3 RELATIONSHIP TO THE CITY OF LAKE ELSINORE GENERAL PLAN

On December 13, 2011, the City of Lake Elsinore City Council adopted a comprehensive update of the City's General Plan. The 2011 General Plan includes the development of land use policies and land use maps to guide the future development of the City of Lake Elsinore. The Downtown Elsinore Specific Plan site is located within the Historic District of the General Plan (GP).



As shown on the General Plan's Land Use Plan (GP, Figure 2.1A) and on the Historic District Land Use Plan (GP, Figure HD-1), the downtown is shown as being within the "Main Street Overlay" with a mix of land use designations which mirror those identified by the Downtown Master Plan that was adopted concurrently with the General Plan. These designations include "Gateway Commercial", "General Commercial", "Business Professional", "Residential Mixed-Use", "Commercial Mixed-Use", "Medium Density Residential", "High Density Residential", "Public Institutional", "Downtown Recreational", "Open Space" and "Floodway".

The General Plan states that: "In the case of specific plan area, the Land Use Plan depicts a Specific Plan Land Use designation for all areas for which there exists an approved specific plan. Each approved specific plan document governs land uses within its designated boundaries." (GP, p. 2-2.) In describing the specific plan land use designation, the General Plan provides that "the designated uses contained in each specific plan are the governing land uses." (GP, p. 2-18.)

Accordingly, adoption of the Downtown Elsinore Specific Plan will necessitate concurrent consideration of a General Plan Amendment. The General Plan Amendment will change the land use designations within the specific plan boundaries to the single land use designation of "Specific Plan."

To ensure that the Downtown Elsinore Specific Plan is consistent with the General Plan, a review of the General Plan's goals and policies was completed and included as Appendix A of this document. Additionally, the General Plan's appendices will be amended to include a summary of the Downtown Elsinore Specific Plan, with several exhibits.

3 LAND USE AND DEVELOPMENT STANDARDS

3.1 INTRODUCTION

This chapter contains standards and guidelines for new development and reuse of properties in the Downtown Elsinore Specific Plan area. The chapter is organized into sections that describe land uses, development standards, design guidelines, architectural styles, signage, and the public realm. The land use regulations and development standards constitute the primary zoning provisions for the Downtown Elsinore Specific Plan area.

3.2 NONCONFORMING USES AND STRUCTURES

The Downtown Elsinore Specific Plan is a blueprint for future development. The Specific Plan requirements apply when an owner proposes to put new development on their property that replaces or significantly expands existing uses or structures. However, the Specific Plan also ensures the right of existing owners and businesses to continue with existing uses. At the time of the adoption of the Downtown Elsinore Specific Plan, if a lawful use of land or structure exists which otherwise would not be permitted by the regulations established by the Plan, such use or structure may be continued subject to the following:

- A. Any use that was lawfully established prior to the adoption of the Plan made non-conforming by such Plan will be allowed to remain (i.e., grandfathered), subject to the provisions in the Lake Elsinore Municipal Code for abandonment of a non-conforming use.
- B. The following uses may continue, and owners will be allowed to make limited physical additions (by no more than 10% of the original square footage), remodel and fully maintain their property during the interim:
 - 1) Single family residence.
 - 2) Attached duplex/four-plex units.
 - 3) Commercial or office/professional use.
 - 4). Civic/cultural use.

- C. All other issues of nonconformance shall be consistent with Chapter 17.164 of the LEMC.

3.3 LAND USES

The Downtown Elsinore Specific Plan offers flexibility for development of a variety of uses that meet the needs of residents while also providing vibrant activities and experiences on Main Street. The Specific Plan area is designated primarily for Mixed Uses, with High Density and Medium Density Residential uses which support commercial and retail business designated along its northern and southern edges.

Starting at the intersection of Flint Street and Main Street, land uses and building design that enhance the City's gateway are encouraged. On both sides of Spring Street, land uses and building design that take advantage of the proximity of the City's Riverwalk are encouraged. In the Downtown Recreational designation at the end of Main Street adjacent to the Lake, land uses and design should support tourist attractions and activities. Civic and other government buildings and facilities are permitted and encouraged in all land use designations.



Downtown Elsinore Specific Plan is composed of the following land use designations (also as shown in Figure 3-1.

General Commercial

This designation provides for retail, services, restaurants, professional and administrative offices, hotels and motels, public and quasi-public uses, and similar compatible uses, or a mix of these uses. The floor area ratio (FAR) for General Commercial uses shall not exceed 2.0 (e.g., a 10,000 square foot commercial parcel could accommodate up to a 20,000 square foot structure).

Mixed Use

This designation provides for a mix of residential and commercial uses within a single

proposed development area, with an emphasis on retail, service, civic and professional office uses. Residential uses are allowed, either as a stand-alone project, or in combination with commercial uses. The FAR for non-residential uses in the Mixed Use designation shall not exceed 2.50. Residential densities allow up to 24 dwelling units per net acre. A density bonus incentive that allows up to 35 dwelling units per net acre is encouraged. The requirements for density bonuses are further discussed in Section 8.2.1



Business Professional

This designation provides for office and administrative uses, light industrial, research and development, office-based firms, including office support facilities, restaurants, medical clinics, public and quasi-public uses, and similar compatible uses. The FAR within the Business Professional designation shall not exceed 0.45.

Medium Density Residential

This designation provides for typical single family detached and attached homes, duplexes, triplexes, four-plexes, courtyard residences, apartments, condominiums, single-family cluster and multi-family residential units, public and quasi-public uses, and similar compatible uses. Residential densities allow up to 18 dwelling units per net acre.

High Density Residential

This designation provides for single-family attached homes, courtyard residences, apartments, condominiums, multi-family residential units, public and quasi-public uses, and similar and compatible uses. Residential densities allow up to 24 units per net acre.

Downtown Recreational

This designation is exclusive to the Downtown Elsinore Specific Plan and is bounded by the Temescal Wash (outflow channel), the Lake Elsinore waterfront, Lakepoint Park, and Lakeshore Drive. The Downtown Recreational designation is established to create a special lakeside recreational environment that is an extension of the historic downtown. This area is intended to maximize the opportunity for citizens and visitors to enjoy the downtown together as a destination, and to capitalize its location adjacent to the Lake. This area is set aside for public and private permanent open space, and allows for passive and active recreation combined with limited retail, dining, entertainment, cultural, and lodging uses. Permitted uses are limited to open space, local parks, passive and active recreation, nature/interpretive centers, hotels, open-air markets, restaurants, water-oriented recreational commercial uses and special events. All commercial facility development is required to have exceptional architecture, site design, and amenities. The floor area ratio for the 19-acre area as a whole shall not exceed 0.35.

Open Space

This designation provides for public and private areas of permanent open space, and allows for passive and/or active private and public recreation.

Public/Institutional

This designation indicates areas owned and maintained by public agencies such as school districts, water districts, utility companies, the County of Riverside and the City of Lake Elsinore.

Floodway

This designation is for the area comprising the “outflow channel” that conveys floodwaters from the Lake.

Downtown Core

The Downtown Core area is located along both sides of Main Street, from Franklin to the southwest boundary of the Plan. Its purpose is to designate that portion of the Downtown Elsinore Specific Plan, which requires specific policies that promote a vibrant, 18-hour activity downtown destination in which to live, work and play.

Policies which apply within the Downtown Core area are:

1. Uses must promote the walkability of the downtown by catering to pedestrians.
2. Uses must have the capability to remain open serving the public into the evening hours.
3. The existing pedestrian scale and historic character of Main Street within the Downtown Core area should be preserved and enhanced when renovating existing structures or adding new development.
3. Storefronts should be designed in a way which invites pedestrians to come in, browse and spend time shopping.
4. Uses are expected to contribute to an overall “experience” for tourists visiting the City and enjoying its amenities such as the Lake, Diamond Stadium, Rosetta Canyon Sports Park, Launch Pointe Resort and Campground, and extreme sports venues like Skydive Elsinore.



**FIGURE 3-1
LAND USE PLAN**



Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

Table 3-1 Land Use Matrix

P = Permitted

C = Permitted with a CUP

T = Temporary, permitted by TUP

-- = Prohibited

Use (Definitions of uses identified herein are located in Section 9.1 - Definitions.)	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public Institutional	Floodway	Additional Regulations
Amusement use	--	--	--	--	--	C	--	--	--	
Antique shop	P	P	P	--	--	--	--	--	--	
Art gallery	P	P	P	--	--	P	--	--	--	
Assembly use	C	C	--	--	--	C	--	C	--	
Automotive repair or service, including tire stores	C	--	C	--	--	--	--	--	--	
Bait shop	--	--	--	--	--	P	--	--	--	
Bar, pub, cocktail lounge	P	C	--	--	--	P	--	--	--	
Bed & breakfast	P	P	--	--	--	--	--	--	--	
Boat dock or pier	--	--	--	--	--	P	--	--	--	
Boat fuel dispensing facility	--	--	--	--	--	C	--	--	--	
Botanical garden	--	C	--	--	--	P	P	P	C	
Car wash	--	--	C	--	--	--	--	--	--	
Caretaker quarters	P	P	P	--	--	P	P	P	--	Permitted only in conjunction with a commercial establishment
Catering service	P	--	P	--	--	--	--	--	--	
Cigar lounge / Hookah bar	C	C	--	--	--	--	--	--	--	
Community center or plaza	P	P	--	--	--	P	P	P	--	
Convention & conference center	C	C	--	--	--	C	--	C	--	
Day care facility (not including Day care, large family or Day care, small family.)	C	C*	--			--	--	--	--	*Day care facilities are not permitted in the Downtown Core.

Use (Definitions of uses identified herein are located in Section 9.1 - Definitions.)	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public Institutional	Floodway	Additional Regulations
Day care, large family and Day care, small family.)		P		P	P					Subject to compliance with the provisions of the LEMC.
Day Spa	P	P	P	--	--	P	--	--	--	
Drive-through establishments	C	C*	--	--	--	--	--	--	--	*Allowed only if a safe and efficient circulation system can be provided completely on site. *Not permitted in the Downtown Core between Heald Avenue and Library Street.
Financial institution	P	P	--	--	--	--	--	--	--	
Food establishment including serving of beer, wine or liquor	P	P	P	--	--	P	--	--	--	When adjacent to or within the same building as residential uses, use must also comply with LEMC Section 17.86.035.
Gasoline Dispensing Establishment (subject to LEMC Section 17.112.090)	C*	C*	--	--	--	--	--	--	--	*In order to prevent a concentration of Gasoline Dispensing Establishments, such establishments are conditionally permitted when located no closer than 1,500 feet from any existing Gasoline Dispensing Establishment. *Not permitted in the Downtown Core.
Farmers Market	T	T	--	--	--	T	--	T	--	
Government building & facility	P	P	P	P	P	P	--	P	--	
Gun and ammunition sales	C	--	C	--	--	--	--	--	--	

Use (Definitions of uses identified herein are located in Section 9.1 - Definitions.)	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public Institutional	Floodway	Additional Regulations
Health, fitness or exercise club	P	P	P	--	--	P	--	--	--	
Heliport	C	C	C	--	--	C	--	--	--	
Home occupation	--	P	--	P	P	--	--	--	--	
Hotel	P	P	--	--	--	P	--	--	--	
Housing, permanent:	--		--				--	--	--	
Apartment unit	--	P*	--	P	P	--	--	--	--	*Permanent housing in the Downtown Core is only permitted in conjunction with a commercial storefront.
Boardinghouse	--	P*	--	P	P	--	--	--	--	
Condominium	--	P*	--	P	P	--	--	--	--	
Duplex, triplex, fourplex	--	P*	--	P	P	--	--	--	--	
Row house/townhouse	--	P*	--	P	P	--	--	--	--	
Kiosk for retail sales	P	P	--	--	--	--	--	--	--	LEMC Section 17.134.037
Library	P	P	--	--	--	--	--	P	--	
Live/work unit	P	P	--	--	--	--	--	--	--	
Massage establishment	P	P	--	--	--	--	--	--	--	
Medical Office	P	P*	--	--	--	--	--	--	--	*Not permitted in the Downtown Core.
Mineral Spa/Hot Springs Spa	P	P	P	--	--	P	--	--	--	Proper screening required
Mortuary	--	--	C	--	--	--	--	--	--	Does not include cremation facilities
Motel	C	C	--	--	--	--	--	--	--	
Museum	P	P	--	--	--	P	--	P	--	
Nightclub	P	C	--	--	--	P	--	--	--	
Office	P	P	P	--	--	--	--	--	--	
Outdoor seating, dining, cafe	P/ C	P/ C	--	--	--	P/ C	--	--	--	CUP for music, sound or entertainment per Sec. 17.134.035
Park/playground	P	P	P	P	P	P	P	P	C	
Parking facility	P	P	P	P	P	P	--	P	C	
Passive open space/natural area	P	P	P	P	P	P	P	P	P	
Pawn shop	P/ C	--	P/ C	--	--	--	--	--	--	CUP required for gun sales.

Use (Definitions of uses identified herein are located in Section 9.1 - Definitions.)	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public Institutional	Floodway	Additional Regulations
Personal service	P	P/C*	P	--	--	--	--	--	--	*A Conditional Use Permit is required in the Downtown Core.
Pet shop	P	P	--	--	--	--	--	--	--	Sales & grooming only; no boarding
Pier	--	--	--	--	--	C	--	--	--	
Professional office	P	P	P	--	--	--	--	--	--	
Recreation facility	C	C	--	--	--	P	P	C	--	
Resource Protection and Restoration	P	P	P	P	P	P	P	P	P	
Retail sale	P	P	P	--	--	P	--	--	--	
Rooftop Uses	C	C	--	C	C	C	--	--	--	
Specialty Food Shop	P	P	--	--	--	P	--	--	--	
Street vendor	--	T	--	--	--	T	--	--	--	
Studio / Workshop	P	P	--	--	--	--	--	--	--	
Swap Meet, Outdoor	T	T	--	--	--	--	--	--	--	
Theater	C	C	--	--	--	C	--	C	--	
Thrift store	P	--	P	--	--	--	--	--	--	
Valet service	P*	P*	P*		P*	P*				*Valet service is only permitted in conjunction with a permitted use.
Wireless communications facility	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	P/C	Pursuant to the requirements of LEMC Chapter 17.186
Other uses that the Director of Community Development finds to be in accord with the purpose of this Chapter and having characteristics similar to those uses listed in this Table.	P, C, T or --									

3.4 DEVELOPMENT STANDARDS

3.4.1 Specific Plan Development Requirements

In order to ensure the orderly and sensible development of the land uses proposed for the Downtown Elsinore Specific Plan, land use planning and design standards have been incorporated into the Specific Plan document. These land use planning and design standards, will assist in accommodating the proposed development and provide adequate transitions to neighboring land uses.

Additionally, project-wide development standards have also been prepared to complement those described throughout the Specific Plan document. The project-wide standards are:

1. The Downtown Elsinore Specific Plan will be developed with a mixed use of residential dwelling units and non-residential land uses, as illustrated on Figure 3-1, Land Use Plan.
2. Standards and guidelines relating to signs, landscaping, parking and other related design elements will conform to the guidelines and standards within the Downtown Elsinore Specific Plan.
3. Where this Specific Plan is silent, the City of Lake Elsinore Municipal Code will apply.
4. Development of the property must be in accordance with the mandatory requirements of the City of Lake Elsinore Municipal Code and State laws; and conform to the Downtown Elsinore Specific Plan.
5. Lots and tentative maps must conform with the Downtown Elsinore Specific Plan development regulations.
6. The creation of new flag lots is discouraged.
7. Qualifying New Developments and Significant Redevelopments are required to develop and implement site specific Water Quality Management Plans (WQMPs). A preliminary project-specific WQMP should be submitted as early as possible during the environmental review and/or planning phase for land use entitlements.

8. Each planning phase should include development of adjacent common open space areas and applicable infrastructure.
9. All private parks should be maintained by a homeowners or property owners association. Public parks must be fully improved by the developer and dedicated as “turn-key” parks to the City.
10. Above-ground pad-mounted equipment and street light poles should not be placed within sidewalks nor block intersection visibility, and above-ground pad mounted equipment should be properly screened with landscaping.
11. The following design concepts within each individual development proposal should be addressed:
 - a. Circulation for pedestrians, bicycles, vehicles, and police patrols.
 - b. Lighting of streets, walkways, parking areas and bikeways.
 - c. Visibility of doors and windows from the street and between buildings, where practical.
 - d. Visual surveillance of open space areas.
 - e. Fencing at appropriate heights and with appropriate materials.
 - f. Buildings constructed with fire-retardant roofing materials.
 - g. Adequate fire flow provided to the development site.
 - h. Connectivity throughout the plan area, as well as to adjacent areas. Of particular importance is connectivity from and to the City’s Riverwalk, and the future Lucerne Bridge.
12. The following crime prevention measures should be considered during the site and building layout design for the security and safety of future occupants of office and commercial space:
 - a. Addresses which light automatically at night and are visible from the street.
 - b. Installation of burglary-resistant hardware, fixtures and burglar alarms in all commercial buildings.
 - c. Special lighting requirements on any buildings that are grouped in a way that individual addresses are difficult to read.
13. Commercial areas and multiple-family complexes should contain covered, bermed enclosures for collection of refuse and recyclable materials and installed full capture trash devices as part of the storm drain system.

14. Passive solar heating techniques should be employed whenever possible within the project. Passive systems involve orienting buildings properly, planting trees to take advantage of the sun, adequate roof overhangs and proper wall insulation.
15. All development within the Downtown Elsinore Specific Plan shall comply with all provisions of the California Green Building Code and the following specific requirements:
 - a. Non-residential developments shall designate 10% of total parking spaces for any combination of low-emitting, fuel-efficient and carpool/vanpool vehicles (consistent with the California Green Building Code). Parking stalls shall be marked "Clean Air Vehicle."
 - b. Non-residential buildings constructed in the Specific Plan shall use roofing materials having solar reflectance, thermal emittance of Solar Reflectance Index (SRI) 3 or better, consistent with CalGreen Tier 1 values.
 - c. All new construction shall be consistent with CalGreen Tier 1 energy-efficient building standards through either the performance based or prescriptive approach described in the California Green Building Code. Alternately, a solar photovoltaic system and/or solar water heating may be used to assist in meeting all or a portion of the 15% requirement.
 - d. Buildings constructed in the Specific Plan will comply with CalGreen Tier 1 indoor water conservation requirements. Indoor water conservation will be addressed and verified at the Design Review stage.
 - e. Development projects shall be required to divert, recycle or salvage at least 65% of non-hazardous construction and demolition debris generated at the site by 2020 (consistent with CalGreen Tier 1, Section A5.408.3.1). All construction and demolition projects shall be accompanied by a waste management plan for the project and a copy of the completed waste management report shall be provided to the Community Development Department upon completion.
 - f. All new multi-family residential, and all non-residential development shall include solar photovoltaic systems that meet at least 50 percent of the development's projected energy use.
 - g. A purple pipe system will be constructed as part of the infrastructure for implementing development projects. Reclaimed water when available will be utilized for irrigation of parks, streetscapes and other landscaped areas.
 - h. All non-residential development shall install electric vehicle charging stations at a minimum of five percent (5%) of its parking spaces.
16. All commercial, recreation and light industrial uses should incorporate buffers that minimize the impacts of noise, lighting, odor, visibility of activity, truck deliveries

and other business activities on adjacent or nearby residential uses.

17. All relevant mitigation measures set forth in the Recirculated Program Environmental Impact Report (State Clearinghouse No. 2005121019) for the City of Lake Elsinore General Plan Update are incorporated by reference and shall be incorporated as conditions of approval for all implementing development projects within the Downtown Elsinore Specific Plan. As a condition of approval for each implementing development project, the developer(s) shall fund the City's implementation of the Mitigation Monitoring & Reporting Program through every stage of development in compliance with procedures established by the City for each implementing development project.

Table 3-2 Development Standards

Development Standards	Land Use Designations									Notes
	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public/ Institutional ¹	Floodway	
MINIMUM LOT SIZE										
Area	None	None	None	7,260 sq. ft.	8,400 sq. ft.	None	None	N/A	None	
Width (Frontage)	None	None	None	60 feet	70 feet	None	None	N/A	None	
Depth	None	None	None	None	None	None	None	N/A	None	
MINIMUM SETBACKS										
Front	0	0	20 feet	20 feet	20 feet	0	N/A	N/A	N/A	
Sides (each)	None; but 15 feet where adjacent to residential use	0	None; but 15 feet where adjacent to residential use	5 feet	10 feet	0	N/A	N/A	N/A	
Street side	15 feet	15 feet	15 feet	15 feet	15 feet	15 feet	N/A	N/A	15 feet	
Rear	None; but 15 feet where adjacent to residential use	None; but 15 feet where adjacent to residential use	None; but 15 feet where adjacent to residential use	15 feet	LEMC R-3 standards	N/A	N/A	N/A	N/A	
OTHER STANDARDS										
Maximum height limit	45 feet	None*	35 feet	30 feet	45 feet	None*	25 feet	N/A	N/A	* The maximum height of a building is not set. However,

Development Standards	Land Use Designations									Notes
	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public/ Institutional ¹	Floodway	
										the building will be subject to floor area ratio requirements that can affect the permitted building height.
Maximum residential density	N/A	24 du/ac*	N/A	7 to 18 du/ac*	19 to 24 du/ac*	N/A	N/A	N/A	N/A	* Up to 35 dwelling units per net acre with Density Bonus Option
Minimum land use area per unit	N/A	N/A	N/A	3,630 sq. ft.	2,420 sq. ft.	N/A	N/A	N/A	N/A	
Minimum dwelling unit size	N/A	See Note 2, below.	N/A	See Note 2, below.	See Note 2, below.	N/A	N/A	N/A	N/A	
Floor Area Ratio (FAR)	2.0	2.50	0.45	N/A	N/A	0.35	N/A	N/A	N/A	Rooftop uses will be exempt from FAR calculations.
Site Coverage	N/A	N/A	N/A	50%	60%	N/A	N/A	N/A	N/A	
Parking ³	1 space per 300 sq. ft.	Sum of Commercial and Residential requirements.	Per provisions of LEMC Chapter 17.148 or as determined by Parking Analysis.	2 spaces per unit. Multi-family projects with more than 10 units: 1 guest space for every 5 units		Per provisions of LEMC Chapter 17.148 or as determined by Parking Analysis.	None	To be determined by Parking Analysis	None	Parking is required only for new construction/ additions. Existing buildings are exempt.

Development Standards	Land Use Designations									Notes
	General Commercial	Mixed Use	Business Professional	Medium Density Residential	High Density Residential	Downtown Recreational	Open Space	Public/ Institutional ¹	Floodway	
Notes: ¹ Public/Institutional uses, as sites for public facilities, are subject to civic planning efforts and public review of proposed projects, rather than specific development standards. ² For Vertical development in the Mixed Use land use designation and for multi-family development in the Medium Density Residential and High Density Residential designations, the minimum dwelling unit size, exclusive of any balcony or patio area, shall be: a. Bachelor dwelling units (sleeping quarters within the living room area): 450 square feet; b. One-bedroom dwelling units: 600 square feet; and c. Two-bedroom dwelling units or larger: 700 square feet plus 100 square feet for each additional bedroom. ³ Additional use specific parking requirements: Hotel – 1 space per room, Office – 3 spaces per 1,000 sq. ft.										

3.5 DESIGN GUIDELINES

3.5.1 General Design Standards and Guidelines

General Design Guidelines are intended to identify key aspects of project design for quality development and re-development within the Downtown Elsinore Specific Plan boundary. Preservation of historic structures and facades is a key objective of these guidelines as historic buildings are viewed as integral to the current and future identity of Downtown Lake Elsinore.

The main access points to Downtown Lake Elsinore will receive special gateway treatment. Three main points of access to the Downtown area are: from the I-15 Freeway to the north along Main Street, from the western neighborhoods of Lake Elsinore from Graham Avenue, and from Lakeshore Drive to the south along Main Street.

3.5.1.1 Gateways

Intent: *To establish a sense of place and celebration of arrival by creating special treatment in the public and private realms of major intersections leading to the Downtown Elsinore Specific Plan Area.*

- Gateway intersections should receive special treatment such as larger building setbacks for increased sightlines, specially treated crosswalks, accent street trees, special paving components, bike racks, seating areas, fountains, signage and special lighting.
- Surface parking should not be located adjacent to corners of gateway locations.
- Treatment in the Gateways should incorporate easements to allow for entry monumentation and distinctive landscape treatment.
- Downtown gateway monumentation is recommended on both sides of Main Street, at its intersection with Flint Street.



3.5.1.2 New Development

Intent: *To create a contemporary look for the Downtown area that is complementary with the existing historic character of Downtown.*

- ❖ New development should be developed with a scale, massing and building articulation of a contemporary design, while at the same time being respectful of and compatible with the character of the historic Downtown.
- ❖ Projects built adjacent to existing lower-scale residential development should respect the scale and privacy of the adjacent properties. Transitions can be made by varying the massing within a project, stepping back upper stories and varying sizes of elements to transition to smaller-scale buildings.
- ❖ New development should promote the creation of new public parking options through public-private partnerships.
- ❖ New development should be planned and designed in conjunction with planned public projects and improvements in the public right-of-way to boost attraction of new businesses to the Downtown area.
- ❖ New commercial development should be sensitive to the community and visitor demands for specialty retail, entertainment, and resort uses by focusing on quality restaurants and food/beverage stores, and action sports-related commercial uses.
- ❖ New development projects should incorporate sustainability practices in buildings and in the public right-of-way, such as drought tolerant landscaping, and energy/water conservation techniques and best practices.



3.5.1.3 Building Placement, Form and Articulation

Intent: *To create compact and walkable neighborhoods in Lake Elsinore's Downtown area.*

- ❖ The interface between the Public and Private realms where a building meets the street should be treated to respond primarily to pedestrian needs and scale.
- ❖ Buildings along Main Street should be designed to create visual interest at the street level using techniques such as staggering the frontage of the building, recessing doors and windows, providing a variety of display windows, and providing awnings and canopies for weather protection and shade.
- ❖ Buildings should be oriented towards the sidewalk and provide unobstructed ADA compliant access for pedestrians from the sidewalk to the building entrance.
- ❖ To create visual interest and minimize the length of flat and repetitive walls, buildings should incorporate techniques such as step backs of upper floors, changes in plane, varied colors and use of architectural elements such as columns, balconies, recessed entryways and windows, and trellises which create shadow lines.
- ❖ Pedestrian bridges over streets should be avoided thereby encouraging pedestrian activity along streets.



3.5.1.4 Building Types

Intent: *To assure that new buildings in the Downtown area are designed and built to reflect their respective function and implement the vision and character of the Downtown.*

Allowable building types in the Downtown area include, but are not limited to, the following:

CULTURAL / CIVIC

- ❖ Mostly single public use
- ❖ Unique contemporary architecture and materials
- ❖ Landscaped setbacks, plazas, special lighting, fountains and other site amenities are incorporated into the design.



MIXED-USE COMMERCIAL

- ❖ Retail on the ground floor and offices or residences on upper levels
- ❖ Shop windows and transparent glass on the ground floor to promote and enhance the pedestrian environment
- ❖ Canopies and recessed entries for weather protection



MIXED-USE CIVIC

- ❖ Mostly public use with some supporting retail on the ground floor
- ❖ Offices or residences on upper levels
- ❖ Façade articulation on the ground floor may create public plazas and points of public gathering
- ❖ Façade articulation and/or architectural accent and details on upper levels



RETAIL PAVILION

- ❖ Mostly occupied by a single retail business such as a supermarket in a one-story building
- ❖ Surface and/or subterranean parking
- ❖ Loading dock screened from views
- ❖ Landscaped setbacks
- ❖ Pedestrian connectivity to street



STOREFRONT RETAIL

- ❖ Primary use is retail
- ❖ One or multi-story buildings
- ❖ Historic context and / or character
- ❖ Located mostly along Historic Main Street
- ❖ Row of buildings of similar scale built adjacent to sidewalks
- ❖ Awnings or canopies provide protection for pedestrians from sun and wind.



COURTYARD RESIDENCE

- ❖ Typically 2-3 story residential buildings
- ❖ Internal courtyard with landscaping and recreational amenities
- ❖ Parking on lower level takes advantage of grade change



LIVE-WORK TOWNHOUSE

- ❖ Typically 2-3 story individual residential units
- ❖ Individual parking spaces
- ❖ Partially underground parking optional
- ❖ Work space on the lower level



3.5.1.5 Open Space

Intent: *To provide attractive landscaped common recreational and passive open space as well as private outdoor spaces for residents of the downtown area.*

PLAZAS, COURTYARDS, PASEOS, AND POCKET PARKS



- ❖ Public Plazas should incorporate recyclable water features especially at gateway locations, to celebrate water as one of the key attractions in Lake Elsinore.

- ❖ Public plazas or parks should allow pedestrian connectivity among adjacent neighborhoods.

- ❖ Public plazas should provide continuous expanses of paved areas to be utilized for special events such as arts and crafts fairs or civic gatherings.



- ❖ Public Plazas and paseos should be well lit for safety, as well as contain regularly spaced electrical outlets to allow for special events that require extra lighting or sound systems.

- ❖ Pedestrian walkways (paseos) leading to courtyards or residential units should

be well lit.

- ❖ Paseos should provide sufficient width to incorporate landscaping, benches, and artwork.
- ❖ Buildings fronting paseos should have windows and doors that lead directly on to the paseo to improve safety at all hours of the day or night.



ON SITE OPEN SPACE STANDARDS FOR MIXED-USE AND MULTI-FAMILY RESIDENTIAL PROJECTS

- ❖ Mixed-Use commercial developments should incorporate courtyards into their site plan to expand outdoor dining opportunities.
- ❖ Open space should be provided for each mixed-use project that contains residential uses.
- ❖ Mixed-use projects should have a goal of providing 200 square feet of open space per residential unit.
- ❖ The open space requirement may be met through a combination of private and public space.
- ❖ The requirement for open space should not be satisfied through the utilization of parking areas, driveways, service areas or unusable slopes.
- ❖ Open space area may be provided by paseos and balconies.



3.5.1.6 Landscaping

Intent: *To assure that the landscaping selection is appropriate for the City of Lake Elsinore in terms of scale, maintenance and aesthetics, and is in accordance with current sustainability standards, measures and guidelines.*

GREEN DESIGN TECHNIQUES

- ❖ Lot coverage should allow an adequate percentage of permeable space such as permeable paving, bio-swales, water retention zones and ponds, and vegetated areas.



- ❖ Sustainable landscape materials that weather well should be used to minimize maintenance and promote sustainability.
- ❖ Water conservation measures such as satellite coordinated sprinklers, gray water irrigation, and local drought tolerant vegetation should be prioritized in all public and private landscaped areas.
- ❖ Green roofs and the utilization of solar panels should be encouraged in the design of new developments.



LANDSCAPING STANDARDS

- ❖ Landscape design must incorporate energy and water conservation concepts, including xeriscape.
- ❖ Landscaping should use drought-tolerant, non-invasive plants and must comply with the provisions of the City of Lake Elsinore’s “Water Efficient Landscape Requirements Ordinance”. (LEMC Chapter 19.08) [Except for temporary seasonal floral and ornamental plants.]
- ❖ Landscaping should be used to provide an attractive setting for development, soften hard building contours, shade walkways and other large expanses of pavement, buffer and merge various uses, mitigate building height; and screen unsightly uses.
- ❖ Landscaping plans should complement the landscape and hardscape elements between the proposed project, surrounding streetscapes, and adjacent publicly maintained landscaping to ensure community continuity and character.
- ❖ Landscaping should be included as part of the design for fences and walls. Plantings should be used to soften and screen large masses of blank wall surface area and to deter graffiti.
- ❖ An anti-graffiti paint coating must be applied to all exposed solid wall surfaces.
- ❖ Larger, more mature plant materials should be used in areas of particular importance such as entries, courtyards and recreational areas to achieve an immediate effect.
- ❖ All landscaping and irrigation systems must be maintained in good condition for as long as the use on the property continues.

PLANTING FOR PARKING AREAS

- ❖ Parking areas should include a surrounding planting coverage area consisting of a combination of shade trees and planting, such as shrubs and ground cover.
- ❖ Planting must be evenly distributed within the parking area, and be placed so as not to impede the safe circulation of pedestrian and vehicular traffic.
- ❖ The landscaping plans should strive to meet the following criteria:
 - One 24" box size tree for every 500 square feet of landscaped area. Trees required for planting islands shall not count toward this requirement.
 - One shrub will be provided per every 50 square feet of landscaped area, and shall include forty percent five-gallon or larger specimens.
 - Rooted ground cover will be included within all landscaped areas, and shall be spaced according to standards requirements of the specific plant utilized.
 - Planting islands will be provided per every eight (8) parking stalls, and shall incorporate a combination of shade trees, shrubs and ground cover.
 - Planting islands should be a minimum of five (5) feet wide and of equal length of the adjacent parking stalls.
 - One 24" box size tree must be provided at each planting island.

3.6 ARCHITECTURE

The architectural character of development within the Downtown Elsinore Specific Plan is intended to be an eclectic blend of both historic buildings and contemporary designs that reflect efficiency, innovation, and uniqueness. This flexibility of design encourages builders to utilize features that best accommodate their use, location and market base. Architectural expression and design will be proposed by the developer and reviewed through the City's Design Review process.

3.6.1 Architectural Requirements

1. **Historic structures** – In accordance with the City’s General Plan, existing and new development should embody the rich historical heritage of the downtown.
2. **Downtown Core** –New structures within the Downtown Core area should reflect the design characteristics and styles that were prevalent in the City from 1880 to 1930. New development, infill development or significant additions to existing buildings should keep the scale and the massing characteristics of the predominant style but may incorporate contemporary elements of other styles that do not compromise the overall character of the area.
3. **City Branding** – Where appropriate and feasible, signage or monumentation should include the City’s “E” logo, the “City of Lake Elsinore Dream Extreme” banner, or other City of Lake Elsinore branding consistent with the City’s approved Identity Standards Guide.
4. **Four-Sided Architecture** – Buildings must provide enhanced architectural features on all four sides. Where a side of a building or a portion thereof is not visible from public view, a waiver of this requirement for that portion that is not visible may be granted by the Director of Community Development during the City’s Design Review process.
5. **Screened Equipment** – Transformers, mechanical or electrical equipment must be screened from public view, and so noted on building elevation plans and/or landscaping plans, if the equipment is ground-mounted. The use of parapets or other architectural features to screen rooftop equipment from ground-level views is encouraged. Where appropriate, painting the equipment to match the rooftop colors is recommended. Screening materials should be similar or complementary to the building materials. Where feasible, integrate utility meters into the architecture; otherwise, they must be screened to minimize views from public areas.
6. **Building Height** – Within the Mixed Use and Downtown Recreational land use designations, the maximum height of buildings is not specifically set. However, the building is subject to floor area ratio (FAR) requirements that can affect the permitted building height. The maximum building height within all other land use designations is set forth in Table 3-2, Development Standards.

3.6.2 Historically Significant Structures

There are many historic or architecturally significant structures in Lake Elsinore. Consideration should be given to preserving and restoring these structures and to

enhancing the architectural character when remodeling.

1. The distinguishing original qualities or character of a building should be preserved. The removal or alteration of any historic material or distinctive architectural features should be avoided.
2. Where feasible, deteriorated architectural features should be repaired rather than replaced. If replacement is necessary, the new material should match the material being replaced in composition, design, color and texture.
3. Alterations that are different yet compatible with the architectural style are acceptable if the design complements the structure in size, scale, color and materials

3.6.2 Architectural Character

Intent: *To encourage the design of buildings which are contextually appropriate to the character where they are located.*

MATERIALS AND EXTERIOR TREATMENT

- ❖ Building materials, textures and colors should utilize contemporary concepts that harmonize with the natural environment and historic character of Downtown Lake Elsinore.
- ❖ Buildings should provide adequate exterior lighting for public safety and enhancement of architectural features at night.



TRANSPARENCY AND FENESTRATION

- ❖ Buildings must provide transparency and adequate fenestration to capture the maximum amount of natural light and ventilation.
- ❖ Retail buildings should provide generous openings at ground level to allow views of display windows by pedestrians and traffic.



- ❖ All glass in windows and doorways at ground level should be clear for maximizing visibility into stores. A minimal amount of neutral tinting of glass to achieve sun control is acceptable if the glass appears essentially transparent when viewed from the outside. Opaque, dark and/or reflective glass should not be used.
- ❖ Windows displays should be placed along paseos.



BUILDING FRONTAGE AND STOREFRONT

- ❖ Retail uses on the ground level of mixed-use buildings should be designed and oriented to capture the highest amount of foot traffic.
- ❖ Buildings should provide frontages that help to define and enliven the Public Realm.
- ❖ Storefronts and building entries should provide canopies or awnings for protection from the weather.
- ❖ Awnings should be coordinated into the overall building façade composition and should not dominate the storefront.
- ❖ Shed awnings with no end panels are encouraged as they provide better visibility into storefronts and appear lighter.
- ❖ Awnings should be composed of solid fabric such as canvas or glass. Metallic canopies are also acceptable. Vinyl awning materials should be discouraged.
- ❖ Entryways to stores should be recessed to ensure that doors do not swing into the public sidewalk.



- ❖ Building entries should create a focus or statement, which can be accomplished by recessed walls and doorways, roof overhangs, canopies, arches, signs, trellises or other similar architectural features. These elements may project into the public right-of-way as long as they do not interfere with public utilities and landscaping.



WALLS, FENCES AND ENCLOSURES

Intent: *To assure that screening and security fencing is designed to complement design intent and character of the surrounding area.*

- ❖ Walls and fences are discouraged unless used to screen a parking lot, or to create a street edge for security purposes.
- ❖ Walls should support vines to minimize places for graffiti.
- ❖ Materials, finishes and colors of walls should complement the materials and design of the main building on the site.
- ❖ Fencing should be treated as an architectural element including the incorporation of art elements and landscaping.
- ❖ The top of walls should not include artwork or barbed wire or other similar material.



Outside Dining Associated with Food Establishments:

- ❖ Establishments located in the Downtown area that provide food services are eligible to apply to the City for an outdoor dining area with enclosure fencing.
- ❖ The height of any enclosure must not exceed forty-two inches.
- ❖ The elevation of the dining area must be the same as the elevation of the sidewalk, and the paving material must not be changed to another material or color.
- ❖ Establishments that serve alcoholic beverages are required to provide an enclosure that meets the requirements of the California Department of Alcoholic Beverage Control, as well as the requirements listed in this document.
- ❖ Enclosures must be capable of being removed without destroying the underlying sidewalk or requiring significant patching of concrete. Devices such as recessed sleeves and posts which may be temporarily covered for use by the next tenant are acceptable. All bolts must be hidden from view.
- ❖ No enclosures are required if dining is limited to one row of tables adjacent to the building.
- ❖ Open metal railings are preferred as outdoor dining enclosures. The design should be compatible with the building façade and the building architectural character. Canvas is not an acceptable material for railings.

- ❖ In all instances where outdoor dining areas are established, unobstructed ADA compliant access for pedestrians must be maintained on public sidewalks.

MIXED-USE BUILDING TYPES CONTAINING RESIDENTIAL UNITS

Intent: *To minimize potential conflicts between commercial and residential uses in mixed-use buildings.*

- ❖ Entrances to residential units shall be separate and distinct from entrances to commercial uses.
- ❖ Mixed-use buildings should be designed so that noise and odors from restaurants, other commercial uses, and their refuse facilities do not affect residential users.
- ❖ Residential units should have adequate noise insulation from non-residential uses in the building.
- ❖ Loading facilities in mixed-use buildings should be located in areas that minimize their effects on residents.
- ❖ A residential unit must have at least one of its required parking spaces dedicated on site.
- ❖ If two residential parking spaces are assigned on site to one residential unit, one space may be in tandem.
- ❖ Guest parking at one parking space per five residential units shall be provided which may be shared with the non-residential uses in the project.

3.7 SIGNAGE

Downtown merchants have different advertising needs than commercial enterprises in other parts of the City. Store fronts can be small and very compact. Signage must be visible from vehicles traveling the street as well as pedestrians strolling the sidewalk. At the same time, sign types, styles, colors and materials play a significant role in achieving the Downtown character. Therefore, the Downtown Elsinore Specific Plan addresses these issues, identifying the appropriate sign types.

3.7.1 General Standards

1. Sign permits must be obtained from the Community Development Director prior to

the placing, erecting, reconstructing, altering or displaying of any commercial signs including change of face or copy, in accordance with the procedures set forth in the LEMC Chapter 17.196.

2. No sign will be permitted that does not pertain directly to an approved business conducted on the premises.
3. All signs must be architecturally integrated with their surroundings in terms of size, shape, materials, color, texture, scale and lighting so that they are complementary to the overall design of the buildings.
4. Signs must be designed with the purpose of promoting retail and street activity while enhancing the pedestrian experience.
5. Permanent signage materials should not include paper, cardboard, or canvas (except for awnings).
6. Signage must not be mechanized with moving parts, nor contain neon flashing lighting.
7. For the health and safety of proprietors, staff and customers, window frontages must be open, with interiors visible from the street. Window signs must be limited to no more than 25% coverage by permanent signs.
8. Permanent signage in the downtown must not include flags, balloons or banners. Flags, balloons and banners are subject to a Temporary Sign Permit in accordance with the regulations set forth in the Lake Elsinore Municipal Code, intended to be displayed or used for a short period of time.
9. A roof sign, erected or constructed wholly or partially above the roofline (the height above the eaves on sloped roofs, or the height above the roof covering on flat roofs) is prohibited.
10. Freestanding signs, those permanent signs that are not attached to buildings such as a pole sign, are prohibited. However, monument signs may be permitted.
11. Signage exempt from these regulations include: store hour signs, wall-mounted menu board signs, credit card signs, health rating signs, temporary political signs, and incidental directional signs.

12. A-frame and sandwich board signs are limited to one only per business and must be located directly in front of the business so as not to impede pedestrian and vehicular traffic. A-frame and sandwich board signs shall not exceed 30 inches wide and 42 inches high and have no more than two faces.



3.7.2 Wayfinding Signs

Strategically located, wayfinding signs guide pedestrians to focal points within the Downtown area or to City points of interest with approximate distance. All wayfinding signs in the downtown will be the same design, color, and font to make them easily identifiable.



3.7.3 Wall signs

A wall sign is any advertising painted on, attached or erected against the wall of a building or structure.

- ❖ Any new signs must respect the current signage patterns established by neighboring businesses.
- ❖ Signs must be erected in a manner consistent with proportion, scale and overall design of the host facade.
- ❖ One wall sign will be permitted per business frontage with a pedestrian entrance.
- ❖ Multi-tenant wall signs will be limited to one per building.
- ❖ The maximum height of a wall sign must be no more than 25% of the height of the building story.



3.7.4 Awnings

An awning is a shelter structure supported from the exterior wall of a building. It may be retractable.

- ❖ The sign area on awnings must be a maximum of 50% of the face area.
- ❖ Periodically, the awning must be replaced when colors fade or materials tear due to weather conditions.



3.7.5 Projecting Signs

A projecting sign is a wall sign which protrudes horizontally from the wall to which it is attached. If the building is single-story, the top of a projecting sign must not be above the cornice or roof line. The bottom of any sign must maintain at least an eight (8) foot pedestrian clearance from the walkway level.



3.7.6 Monument Signs

A monument sign is a ground-mounted, low-profile, free-standing sign with a solid, decorative base composed of brick, concrete, metal, architecturally-treated wood or other similar materials.



- ❖ Monument signs will be erected outside the public right-of-way including sidewalks.
- ❖ There will be only one monument sign per building, but may contain multiple names of businesses within the building.
- ❖ The maximum height of monument signs is 5 feet.

3.7.7 Tower Signs

A tower sign is incorporated into a built structure with more than two faces and extends above the street wall height.

- ❖ A tower sign must not extend beyond the top of a tower.
- ❖ A tower sign must not project more than 3-inches from the wall of the tower.



3.7.8 Window Signs

A window sign is any advertising placed on the interior of a window, or painted on a window such that it can be read from the outside of the building.

- ❖ Permanent and temporary window signs must not cover more than 25% of the area of each window.



3.8 PUBLIC REALM

The Public Realm is the inter-connective fabric of downtown where people can gather and interact with other people and the environment. It is this interaction that begins to create the sense of place, shaping its unique character and ultimately making it memorable.

The Public Realm includes:

- ❖ Public Art
- ❖ Sidewalks and crosswalks
- ❖ Parks and plazas
- ❖ Paseos
- ❖ Landscape and hardscape features including street trees, planters, shrubs or fencing with a maximum height of 42 inches.
- ❖ Signage
- ❖ Lighting
- ❖ Shade structures
- ❖ Street furniture
- ❖ Piers and boardwalks

3.8.1 Landscaping

Landscaping is required with every new development, in accordance with the Lake Elsinore Municipal Code Sections 17.44.060 and 17.112.060, (LEMC) and requirements as specified in this Plan. All landscape plans must be reviewed based upon variables such as location, site design, and compatibility with adjacent development.

1. Landscape design must incorporate energy and water conservation concepts, including xeriscape.
2. Landscaping must be drought-tolerant and comply with the Lake Elsinore Water Efficiency Ordinance (LEMC Chapter 19.08).
3. Landscaping must be used:

- ❖ To provide an attractive setting.
 - ❖ To soften hard building contours.
 - ❖ To shade walkways and other large expanses of pavement.
 - ❖ To buffer and merge various uses.
 - ❖ To mitigate building height. Vines, espaliers and potted plants on walls and columns create visual interest, add texture and accentuate entryways.
 - ❖ To screen unsightly uses or accessory structures.
 - ❖ To soften and screen large masses of wall surface and to deter graffiti. Anti-graffiti paint coating must be applied to exposed solid wall surfaces.
 - ❖ To accent entries, courtyards and recreational areas. Larger, more mature plant materials should be used in these areas to achieve an immediate effect.
4. The developer and subsequent property owner will be responsible for maintaining the landscaping as shown on the approved plans. Maintenance includes regular irrigation, weeding, fertilizing, pruning and replacement of dead materials in accordance with the LEMC.
 5. Trees should be planted in parking lots with the goal of providing a 50% shade canopy coverage within five years after planting. All trees within parking areas should be a minimum 15-gallon size at planting. However, larger trees (e.g., 24-, 36-, and 48-inch box) may be required by the Planning Commission in certain circumstances. Deciduous trees should be considered to provide solar control during summer and winter.
 6. Mature, significant palm trees must be preserved in the City per LEMC Section 5.116. Mature street trees may be replaced with new mature box trees with the approval of the Community Development Director.
 7. Street trees can play a large part in enhancing the character of the downtown, particularly Main Street. The existing California Sycamores (*Plantanus racemosa*) along Main Street are recommended to be retained, with selective pruning and thinning when necessary. Other recommended street trees for the downtown include the London Plane “Bloodgood” (*Platanus acerifolia*), Chinese Pistache (*Pistacia chinensis*), Chinese Flame (Koelreuteria bipinnata), and Jacaranda (*Jacaranda minosifolia*).

3.8.2 Parks & Town Squares

Plazas should be the result of careful planning, and not simply a leftover space between structures. Building grouping is encouraged to produce public outdoor spaces such as plazas, food courts, courtyards, gathering or play areas. Ground-level shops, restaurants, and parking garage entrances are excellent uses surrounding plazas. Building design must include public access and windows that face public spaces. Sun access and wind protection must be carefully considered to ensure that public spaces are used year-round. For the comfort of residents and visitors, trees, awnings, wind breaks, shade structures and other elements should be incorporated as needed. Each park or town square must contain an iconic structure, such as a water garden, water fountain, textured pavement, or public art, which identifies the park or town square.

The Plan's frontage on the Lake provides a great opportunity to promote public access amenities, such as a promenade, which capitalizes on distinctive views of the Lake and the Santa Ana Mountains. Visual cues such as lighting, water features, sculptures and landscaping, can announce the amenities at the Lake edge.

3.8.2.1 Community Plaza Concept

The Downtown Elsinore Specific Plan provides an opportunity to create a unique Community Plaza on Main Street, starting at the intersection of Heald Avenue and Main Street, and extending west to Peck or Graham or Sulphur. The loss of parking spaces is compensated by the construction of the City's Downtown Cultural Center Parking Lot. The closure of Main Street can be permanent, temporary, or a phased project.



1. Permanent closure would enable a design with decorative pavement, landscaping, outdoor furniture, play areas, fountain, the provision of electrical outlets, the location of vending or informational kiosks, and the expansion of restaurants with permanent outdoor seating/serving areas in the Plaza. The beneficial effect to community residents and visiting tourists is to provide a gathering place, a focal point, and pedestrian-oriented amenity very rare in urban settings.

2. Temporary closures with the use of lighted bollards or other removable devices can be targeted for large, community events, such as the WinterFest, Rod Run, or other activity. An occasional closure would divert traffic to outlying streets, such as Spring Street, and the inconvenience of doing so would be sporadic during the year.
3. The phasing of a Main Street closure is a cautious approach to measure the longer-term effects upon the lives of residents and merchants. This approach allows for an evaluation of the benefits and hindrances over an extended period before a decision regarding any permanent closure.
4. An assessment of existing on-street parking located on the affected portions of Main Street shows that closures of Main Street would eliminate the following number of parking spaces:

Spaces between Heald and Peck	22
Spaces between Peck and Graham	22
Spaces between Graham and Sulphur	22

The Downtown Cultural Center Parking Lot anticipates 170 new spaces.

3.8.3 Outdoor Furniture

Public seating areas are encouraged along the Lake edge, as well as throughout the downtown area that promote pedestrian activity. Benches, trash receptacles, news racks, bus shelters, bike racks all add to the enjoyment of visitors to the downtown. Outdoor furniture should give scale, structure and identity to the urban fabric. Outdoor furniture design and materials must reduce as much as possible the potential for damage resulting from vandalism. Decorative deterrents to skateboard vandalism should be incorporated into the design of public areas.

Figure 3-2 Typical Street Furnishings



Figure 3-3 Typical Street Furnishings



3.8.4 Public Art

Art generates interest and variety in the City's streetscape and promotes an enhanced public realm. It can serve as a symbol and identifier, which connects the community and visitors to the culture and history of the City. Several art projects already exist throughout the downtown including murals celebrating Elsinore's 120th anniversary and the old Santa Fe train depot.



3.8.4.1 Public Art Policies

- ❖ Locate public art in strategic locations such as gateways or as focal points in public areas such as parking lots, plazas, parks and paseos/trails.



❖ Smaller permanent art installations such as murals, sculptures and fountains should be encouraged in private properties and in public and cultural buildings.

❖ Temporary installations/exhibits and performance art should be encouraged in public open spaces. The City could allow emerging artists to display their art within public areas as part of public events.

❖ Artwork could be contemporary sculptures or artist designed street furniture, lighting, and paving as well as art created by local schoolchildren to be inserted into public plazas.

❖ Murals are a form of public art that can be used to celebrate the history and culture of Lake Elsinore.



Source: Lake Elsinore's artist Bruce McMeans
Kid's Walk Mosaic
Demo Project

3.8.4.2 Contribution to Artwork Program

Through the Downtown Elsinore Specific Plan, the City encourages future developments, both public and private, to contribute to the Artwork Program. A recommended contribution amount is 1% of the cost of construction of a project. Funding will be used for acquisition and/or installation of art at or near the construction site. Other funding sources such as grants and sponsorships will be directed to support this Program. Both permanent and temporary artworks will be implemented, including art exhibitions, galleries and workshops, and vacant storefront art programs.



4 MOBILITY

4.1 INTRODUCTION

A concept known as “Complete Streets” has gained momentum in the United States during the 21st Century. The Complete Streets concept seeks to change long-standing standard street design requirements that predominantly considered the needs of motorists at the expense of other users. Complete Streets are streets that are planned, designed, operated, and maintained to provide safe mobility for all users, including bicyclists, pedestrians, transit vehicles, and motorists.

The Downtown Elsinore Specific Plan incorporates Complete Streets concepts to create a pedestrian-friendly downtown core with a multi-modal streetscape where there exists accessible and equitable transportation choices for people who live, work, or shop in downtown Lake Elsinore.

4.2 PEDESTRIAN-FRIENDLY DESIGN

Several design components of future development can ensure the creation of a walkable environment. The following factors are highly recommended, and each development plan in the downtown will be reviewed and encouraged to contain them:

4.2.1 Interconnectivity

1. Pedestrian circulation routes around and through any project must be clearly defined and implemented using landscape and site design elements where feasible. Mid-block street crossings are preferred to avoid conflicts with the turning movements of vehicles at intersections.
2. Limit the number and width of sidewalk curb cuts, particularly on Main Street, to minimize pedestrian-vehicular conflicts.
3. Spaces between the sides of buildings should incorporate seating areas for enhanced pedestrian connections where appropriate.
4. Pedestrian access from residential facilities into commercial areas is encouraged through the use of restricted access pedestrian gates that facilitate access for residents to commercial services.

5. Walkways over long distances of pavement must provide landscaping and amenities where feasible for pedestrian rest breaks, visual interests, and protection from the sun and wind.
6. Right-sizing of streets is encouraged to reduce the number of vehicle travel lanes that a pedestrian must cross. If infeasible, then landscaped pedestrian refuge areas must be provided mid-crossing.
7. Bike racks should be provided at convenient locations in the downtown. In the commercial areas, bike racks must be accessible and safe, within well-lighted and visible locations.
8. A Bike Share Program is encouraged to connect the downtown to the Outlet Center, the Diamond Stadium, the Launch Pointe Recreation Destination & RV Park, and other points of interest in the City.
9. A trolley service is encouraged to connect the downtown to the Outlet Center, the Diamond Stadium, the Launch Pointe Recreation Destination & RV Park, and other points of interest in the City.
10. A water taxi station is recommended at the end of Main Street to connect the downtown to the Diamond Stadium, the City's Launch Pointe Recreation Destination & RV Park, campgrounds, and other points of interest around the Lake.

4.2.2 Safety & Lighting

Lighting plans for future development must be reviewed and approved by the Community Development Director prior to issuance of building permits or as otherwise requested. The following requirements must be included in the lighting plans:

1. Lighting fixtures must reflect the style and character of the development.
2. Lighting must be installed to create a sense of security and encourage nighttime activity.
3. Lighting levels for outdoor illumination will be required to meet the minimum standards required for safety. All exterior lighting will be required to be controlled by timers or light sensors, and unless otherwise required, only lighting required for parking lot security and safety will be provided at night.
4. Light fixtures must be carefully located, positioned, and shielded to minimize unwanted spillover and glare.
5. Building lighting that promotes the building's identity or highlights its architecture

and/or landscape features is encouraged.

6. Innovative lighting technologies are encouraged. High efficiency lighting, such as LED, will be utilized for traffic, street and other outdoor lighting.
7. Light fixtures should be pedestrian-scaled, except in parking lots where higher intensity lighting is required.
8. Light fixtures must be vandal proof, made with break-resistant plastic, recessed, or otherwise designed to reduce damage.
9. Project entries must be creatively and well lit to develop a sense of place and arrival.
10. Promenades and walkways must be well lit to ensure safe walking environments. Bollards and pathway lights should be used to illuminate pedestrian links.
11. Security lighting must be installed in secluded areas and other potentially unsafe locations, where feasible
12. Low-voltage lighting conserves energy and should be used in landscaped areas, unless security is important.

4.3 CIRCULATION

4.3.1 Vehicular Circulation

The circulation system within Downtown implements the Circulation Element of the Lake Elsinore General Plan in the Specific Plan area by extending and integrating with the existing street network of the City. The Circulation system improves traffic flow by providing alternative routes in and around the City and by improving access to Interstate 15. The Circulation system is designed to distribute traffic evenly among the various points of ingress/egress of the Downtown to minimize off-site impacts. Figure 4-1, Conceptual Circulation Plan, shows the general location and alignment of the streets within the Downtown Elsinore Specific Plan.

The Circulation system features a hierarchy of street types, including Urban Arterials, Major, Collector, and Local Streets. The development of this hierarchy system results in the creation of a safe and efficient street network in which regionally-oriented traffic is separated from local neighborhood traffic. The Urban Arterials and Majors primarily carry higher volumes of traffic based on regional needs, leaving the collector and local streets to carry smaller traffic volumes serving individual neighborhoods and residences. As a result, the residential areas become quiet places providing a pleasant and desirable

neighborhood environment with minimal traffic intrusion.

4.3.2 Extension of Main Street to the Lake

One of the objectives of the Downtown Elsinore Specific Plan is to “[e]nhance connectivity from the downtown to the lake with the realignment of Main Street, Lakeshore Drive and Library Street.” (p. 1-5, Objective 3) Implementation of this objective will require the City to:

1. Realign Main Street so that it runs straight to Lakeshore Drive.
2. Connect old Main Street to Library Street.
3. Create a larger and revitalized City Park with land lost due to realignment of Main Street on other side of existing park.
4. Purchase land for new right-of-way.

4.3.3 Standard Streets and Cross-Sections

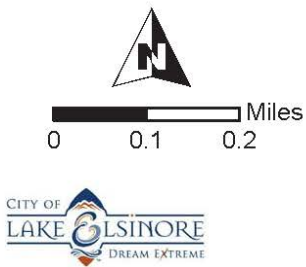
4.3.3.1 Street Hierarchy

The streets within Downtown Lake Elsinore create the framework that will make downtown a connective, walkable, lakefront environment. This framework is composed streets and blocks that have been organized in order to:

- ❖ Maximize density and land use opportunities.
- ❖ Enhance connectivity to neighborhoods adjacent to downtown.
- ❖ Improve mobility within downtown.
- ❖ Establish a character for the downtown.
- ❖ Employ sustainable practices.
- ❖ Link “green streets” to the Outlet Channel.
- ❖ Preserve a riparian, linear park corridor that connects downtown to the lake.
- ❖ Provide a memorable and quality sequence through downtown from the I-15 Freeway to the lake.
- ❖ Celebrate the creation of “great streets” within downtown.



FIGURE 4-1
CONCEPTUAL CIRCULATION PLAN



Prepared by:
City of Lake Elsinore GIS
August 7, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

4.3.3.2 Urban Arterials

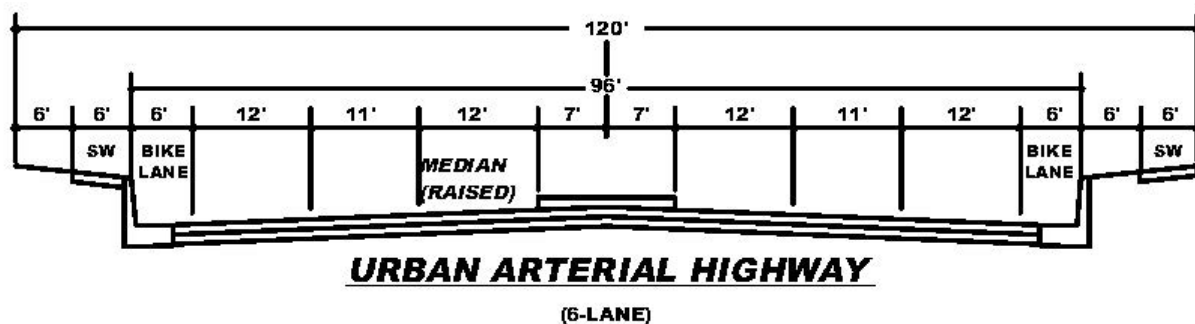
The Urban Arterials are designed primarily to move regionally-oriented traffic safely and efficiently. The Urban Arterials have a right-of-way dimension of 120 feet and feature three travel lanes in each direction separated by a raised and landscaped median. Single- or double-lane left-turn pockets are provided at intersections where warranted. In addition, a landscaped parkway is provided.

There is no designated Urban Arterial within the Downtown Elsinore Specific Plan; however, the following Urban Arterial connects to the Downtown Elsinore Specific Plan at its southeast corner:

❖ Lakeshore Drive

Figure 4-2, below illustrates the typical cross-section of the Urban Arterials within the City:

Figure 4-2 Standard Urban Arterial Cross-Section



4.3.3.3 Majors

The Majors are designed primarily to move moderate volumes of traffic and to provide connections from residential neighborhoods to the Urban Arterials.

The Majors have a right-of-way dimension of 100 feet and feature two travel lanes in each direction separated by a raised and landscaped median. Single-lane left-turn pockets are provided at intersections where warranted. In addition, a landscaped parkway is provided.

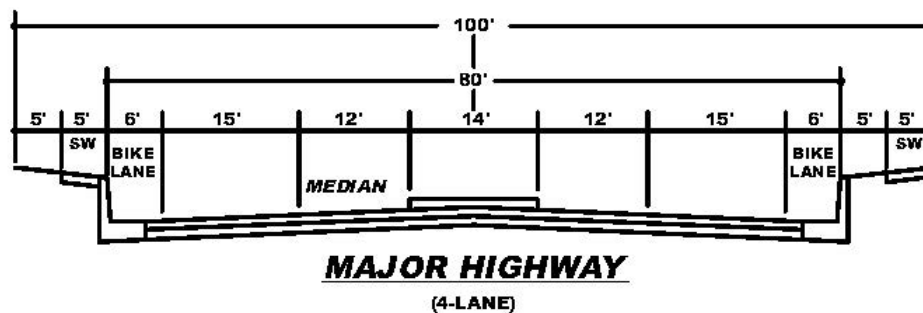
There is no designated Major Street within the Downtown Elsinore Specific Plan;

however, the following Major connects to the Downtown Elsinore Specific Plan at its northern end:

- ❖ Main Street from Interstate 15 to Flint Street

Figure 4-3, below, illustrates the typical cross-section of the Major streets within the City:

Figure 4-3 Standard Major Street Cross-Section



4.3.3.4 Great Streets

These streets serve as major access points into downtown from the I-15 Freeway, to downtown adjacent neighborhoods, and along the lake. Great Streets have the ability to create the most memorable experiences within downtown. They set the standard for the quality of the public realm.

- ❖ Main Street
- ❖ Lakeshore Drive (also a Green Street)
- ❖ Graham Avenue (also a Green Street)

"A great street should be a most desirable place to be, to spend time, to live, to play, to work, at the same time that it markedly contributes to what a city should be. Streets are settings for activities that bring people together." — from *Great Streets* by Allan Jacobs (1995)

Figure 4-4 Main Street Cross-Section – Flint Street to Heald Avenue

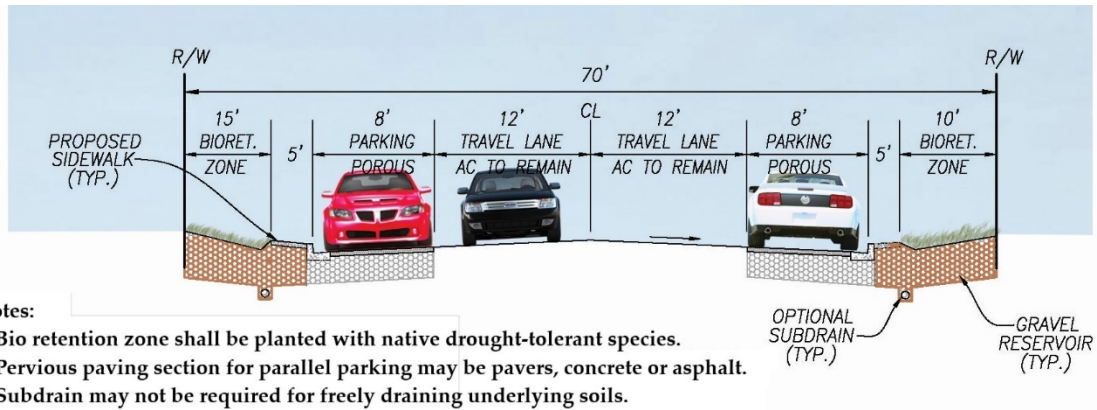


Figure 4-5 Main Street Cross-Section –Heald Avenue to Sulphur Street

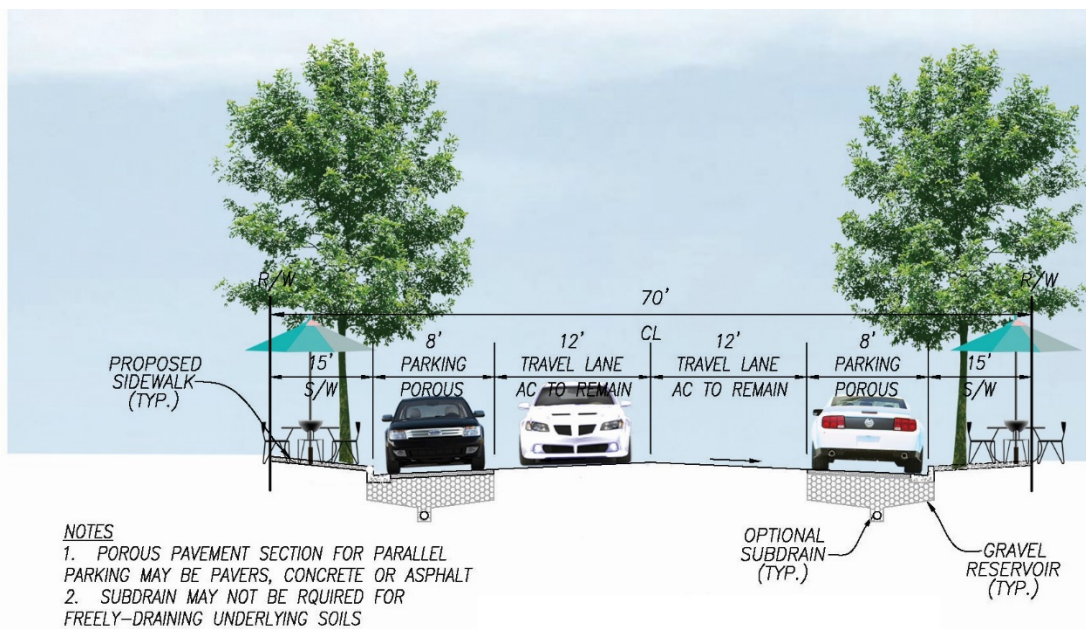


Figure 4-6 Main Street Cross-Section – Sulphur Street to Lakeshore Drive

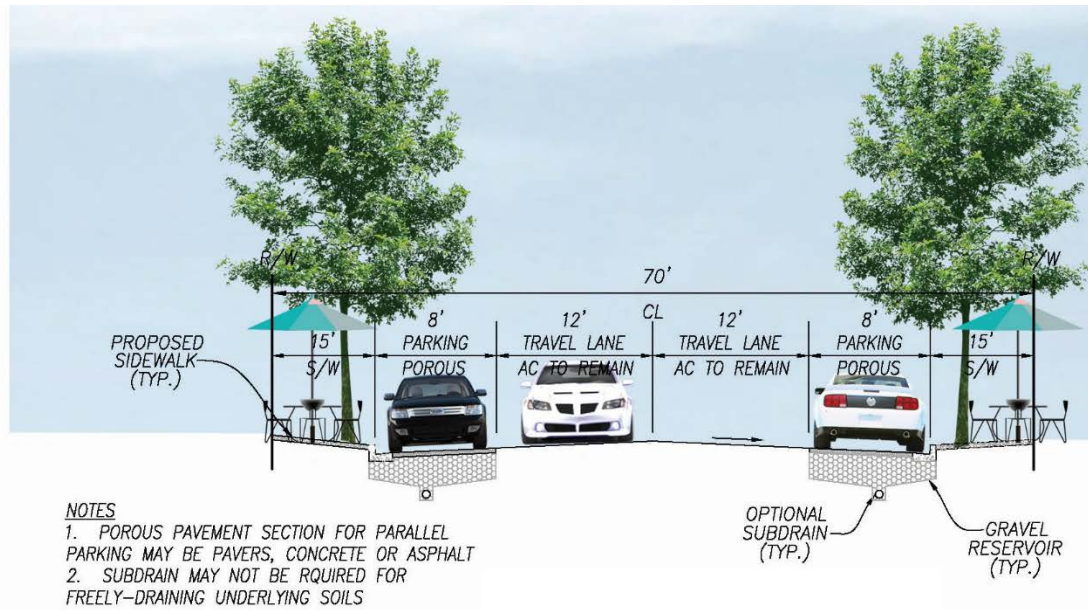
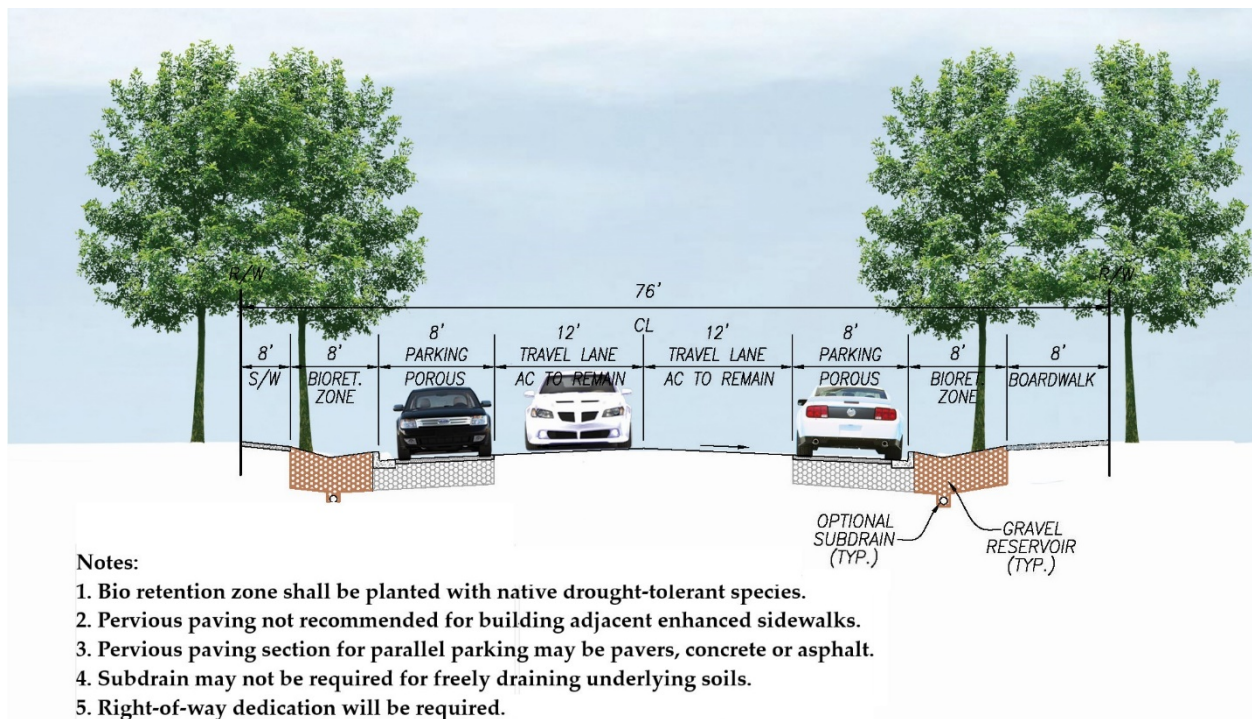


Figure 4-7 Lakeshore Drive Cross-Section

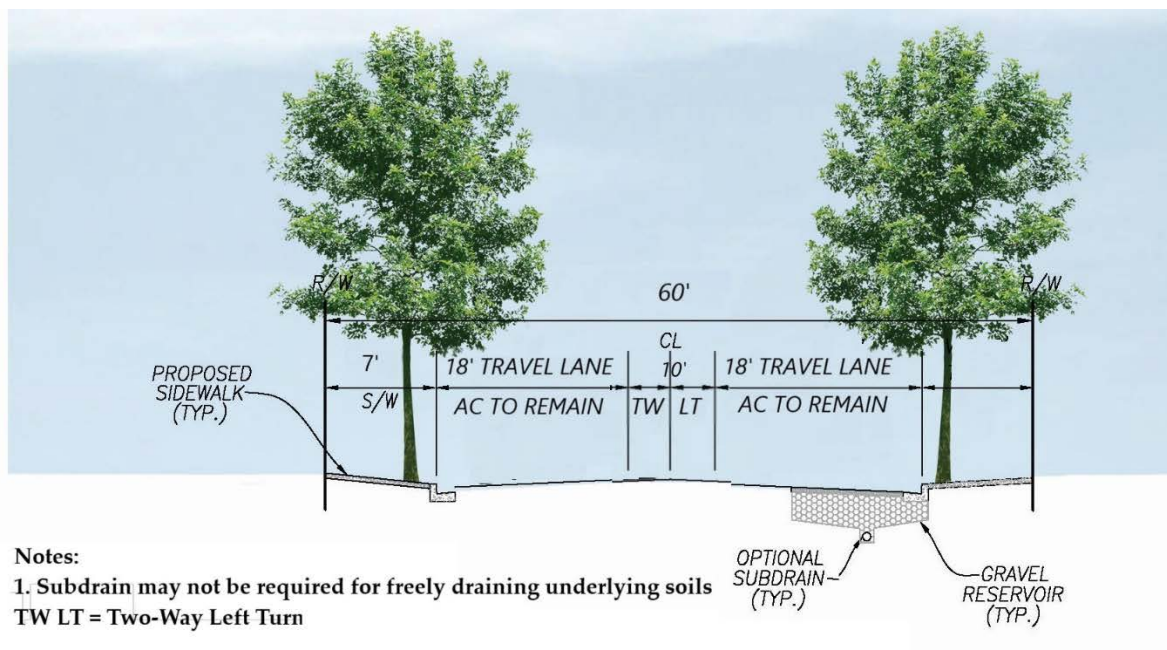


4.3.3.5 Service Streets

Service Streets serve as a business route that carries heavy traffic for business and commercial activities in downtown and allows for Main Street to have the ambience of a Great Street.

- ❖ Spring Street

Figure 4-8 Spring Street Cross-Section



4.3.3.6 Green Streets

These streets operate as “green” corridors that connect downtown adjacent neighborhoods to downtown, and vice versa. Green Streets create a strong link to the Outlet Channel utilizing bioswales for storm water management, shade from trees for comfortable streets and sidewalks, and other sustainable practices to improve environmental quality of life issues.

- ❖ Flint Street
- ❖ Pottery Street
- ❖ Sumner Avenue
- ❖ Heald Avenue
- ❖ Graham Avenue

- ❖ Limited Avenue
- ❖ Lakeshore Drive

Figure 4-9 Graham Avenue and Heald Avenue Cross-Section – Main Street to Outlet Channel

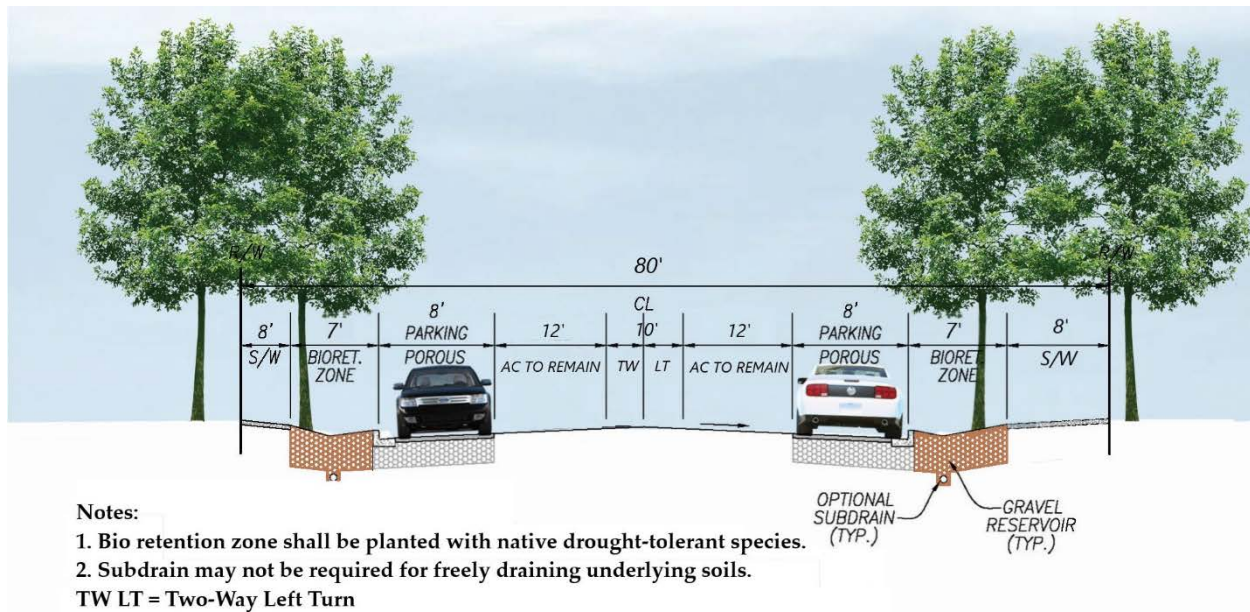


Figure 4-10 Graham Avenue Cross-Section – Main Street to Chestnut Street

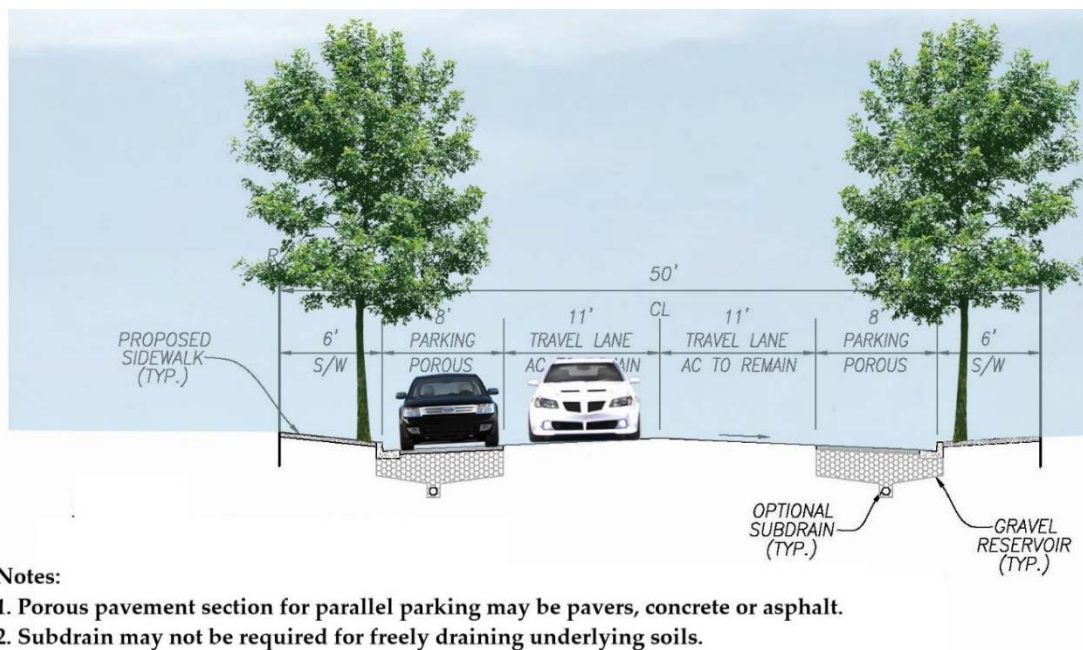


Figure 4-11 Sumner Avenue

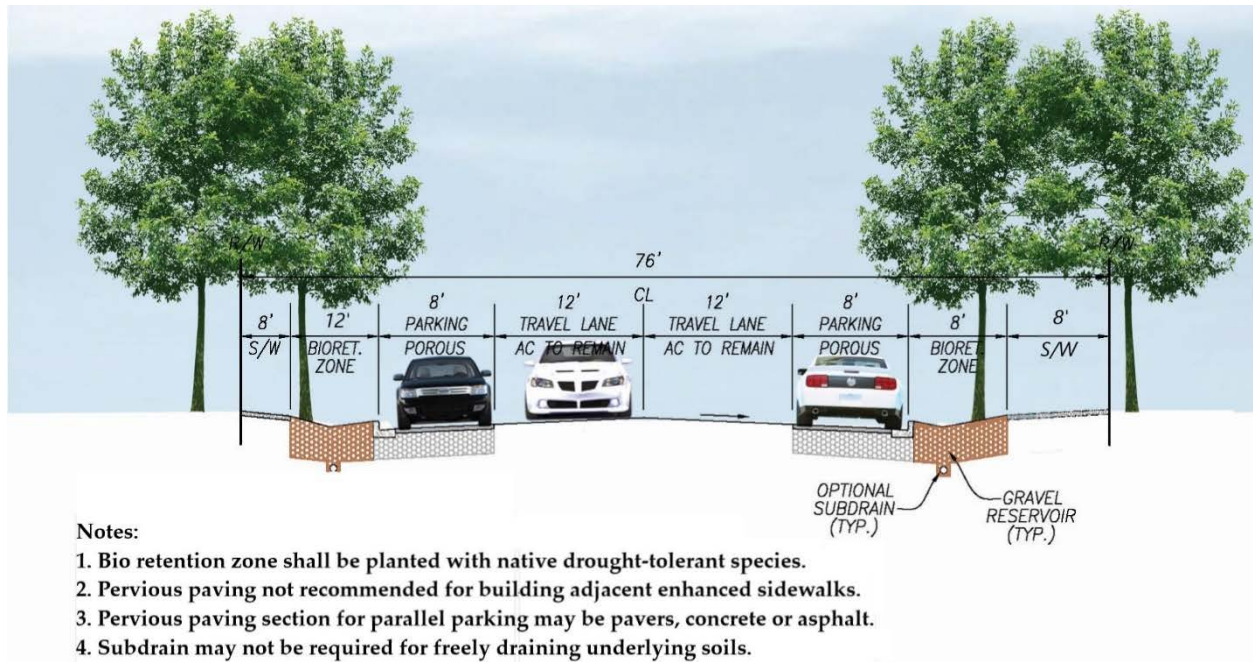


Figure 4-12 Flint Street, Pottery Street and Limited Street Cross-Section

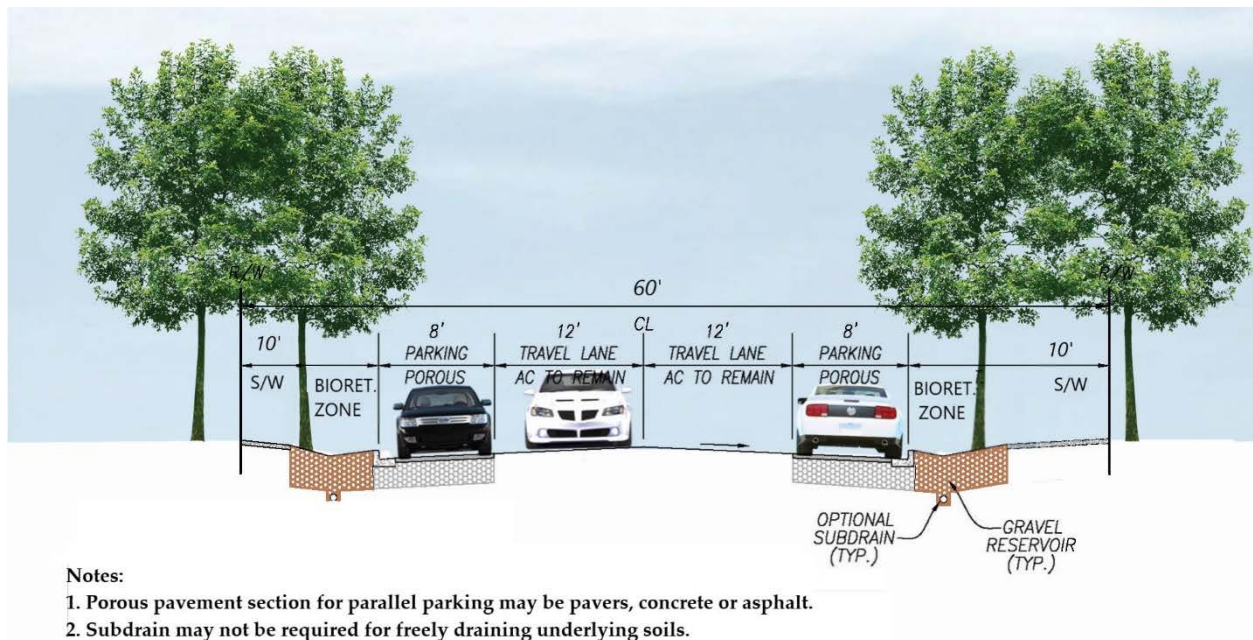
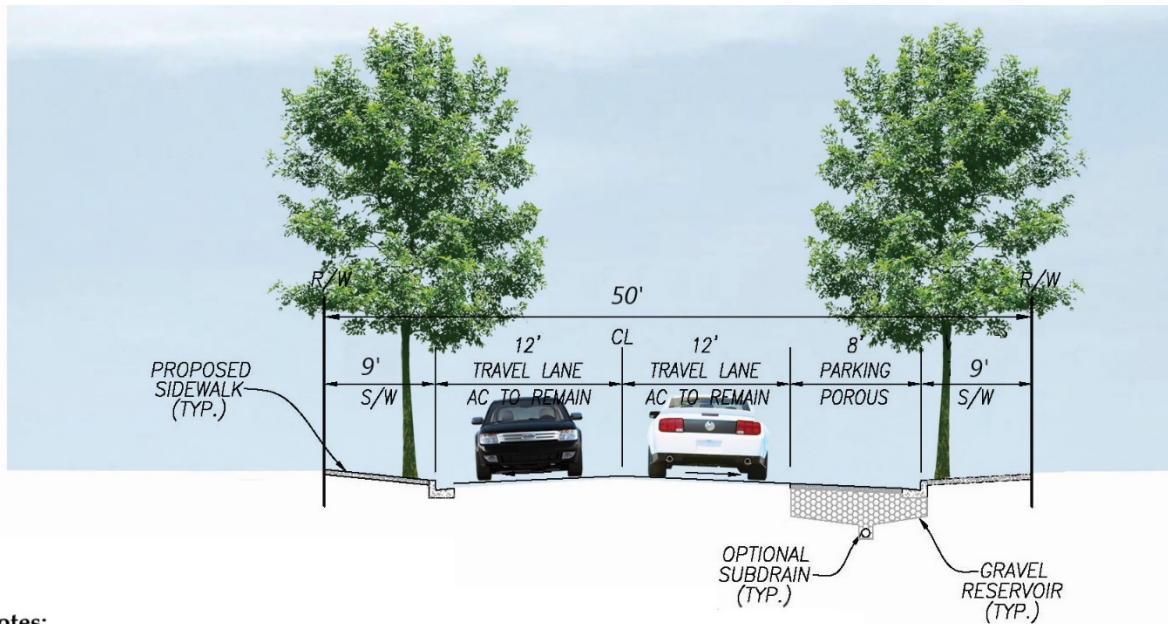


Figure 4-13 Heald Avenue Cross-Section – East of Main Street



Notes:

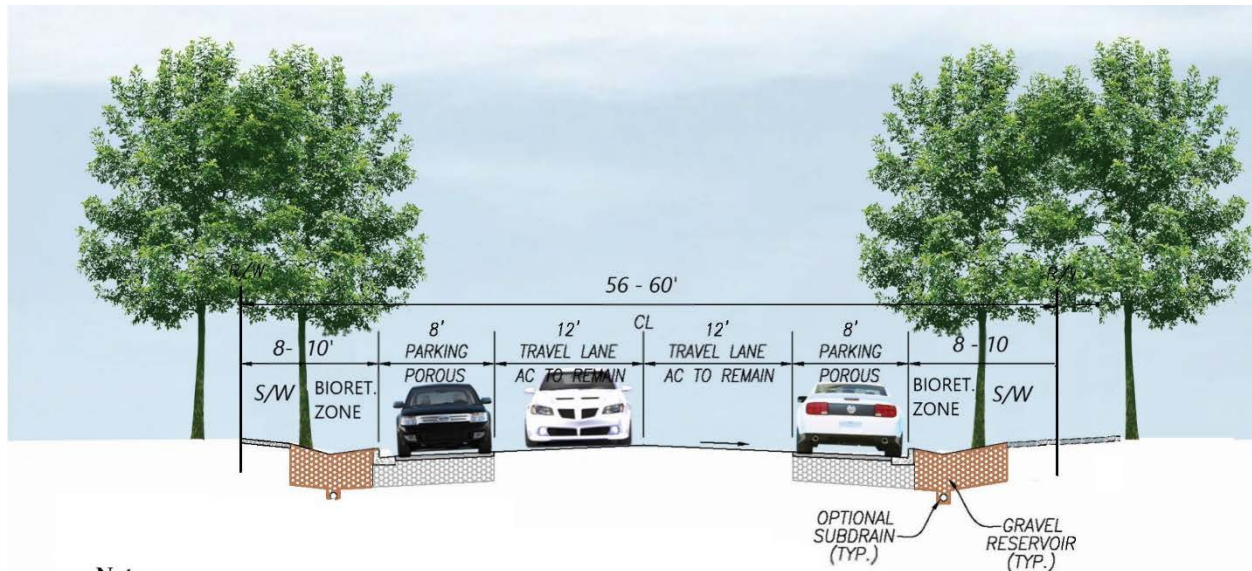
1. Porous pavement for parallel parking may be pavers, concrete or asphalt.
2. Subdrain may not be required for freely draining underlying soils.
3. Wide side of street to be determined.

4.3.3.7 Civic Streets

Civic Streets provide access to important civic open spaces, both City Park and the Circle Park. These streets should be designed to celebrate and accommodate the needs of each of these major public spaces in downtown. These streets may have special paving to denote the arrival into these important spaces.

- ❖ Franklin Street
- ❖ Limited Avenue
- ❖ Library Street

Figure 4-14 Franklin Street, Limited Street and Library Street Cross-Section



Notes:

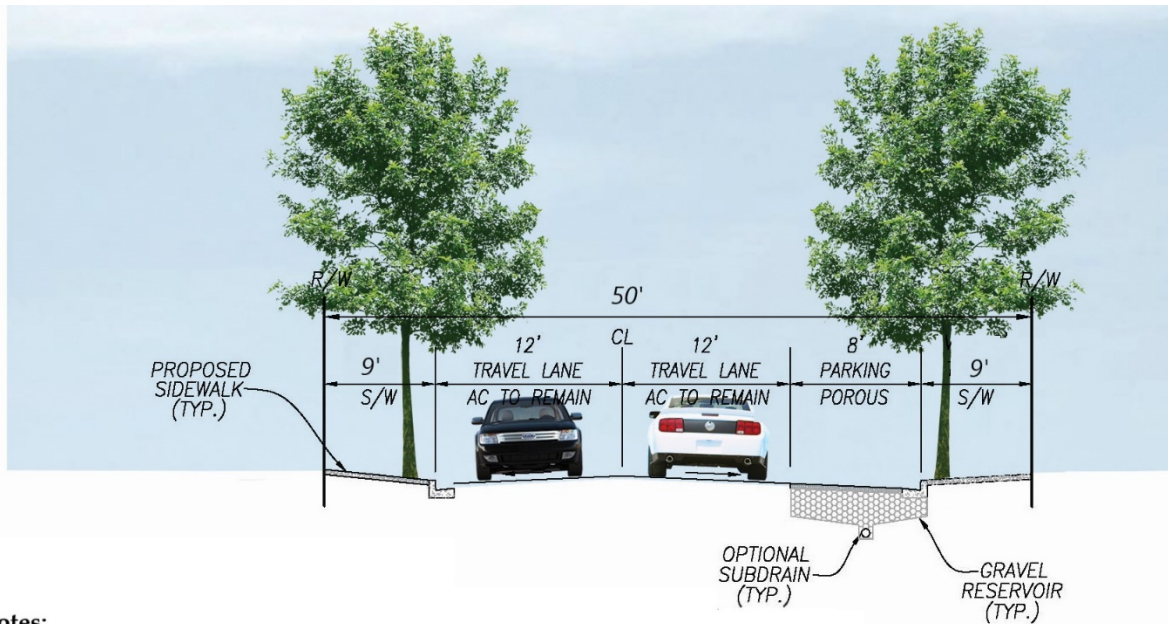
1. Bio retention zone shall be planted with native drought-tolerant species.
2. Porous pavement section for parallel parking may be pavers, concrete or asphalt.
3. Subdrain may not be required for freely draining underlying soils.

4.3.3.8 Edge Streets

Edge Streets create the boundary edge for downtown and should be articulated to create a transition between downtown and the adjacent neighborhoods.

- ❖ Riley Street
- ❖ Ellis Street
- ❖ Chestnut Street

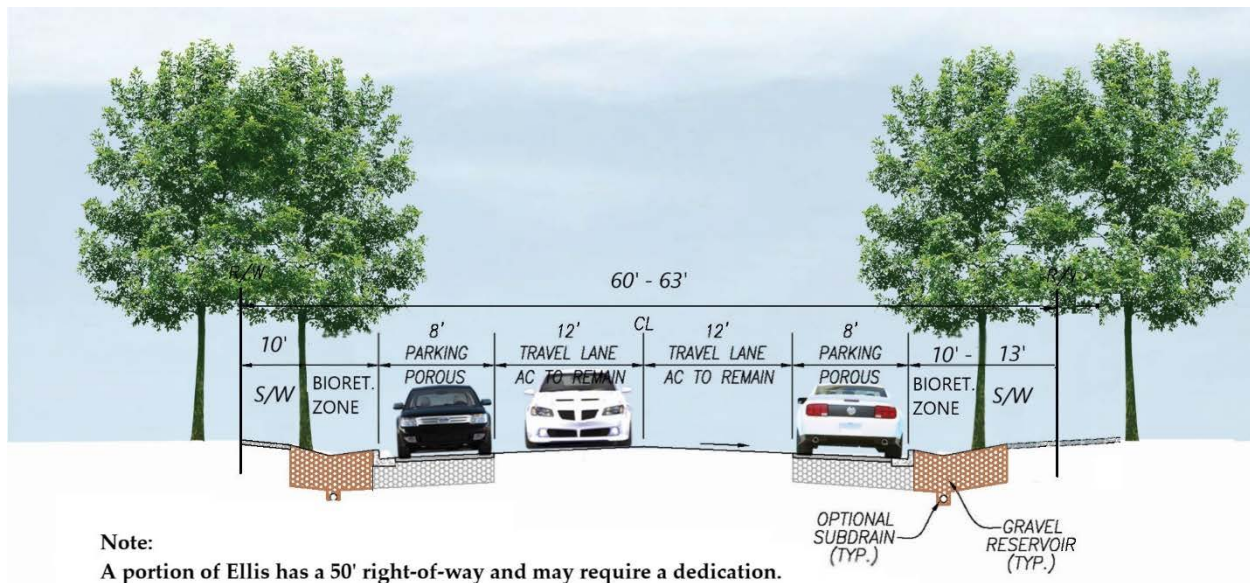
Figure 4-15 Chestnut Street Cross-Section



Notes:

1. Porous pavement for parallel parking may be pavers, concrete or asphalt.
2. Subdrain may not be required for freely draining underlying soils.
3. Wide side of street to be determined.

Figure 4-16 Riley and Ellis Streets Cross-Section



Note:

A portion of Ellis has a 50' right-of-way and may require a dedication.

4.3.3.9 Neighborhood Streets

These streets are the most private streets in downtown, and as a result, they carry less traffic than the other streets and naturally slower cars due to their narrow, intimate nature.

- ❖ Peck Street
- ❖ Prospect Street
- ❖ Sulphur Street
- ❖ Olive Street

Figure 4-17 Peck Street Cross-Section – West of Main Street and Sulphur Street

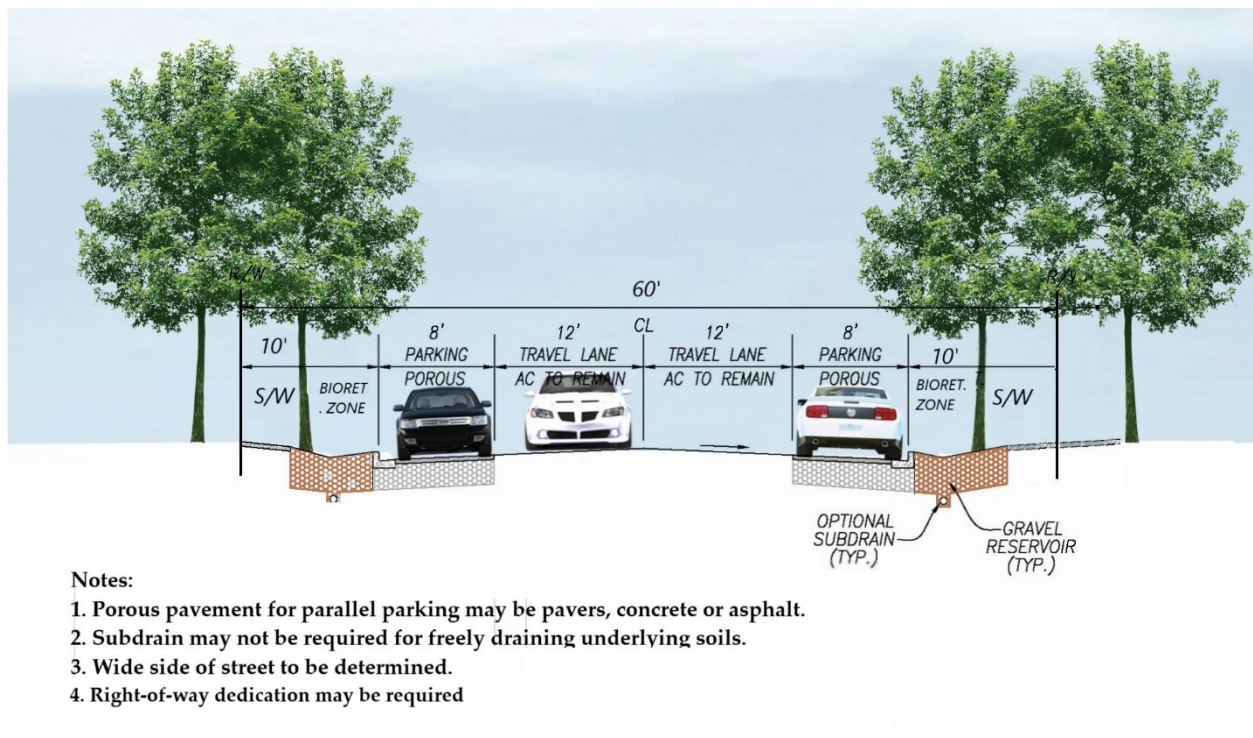
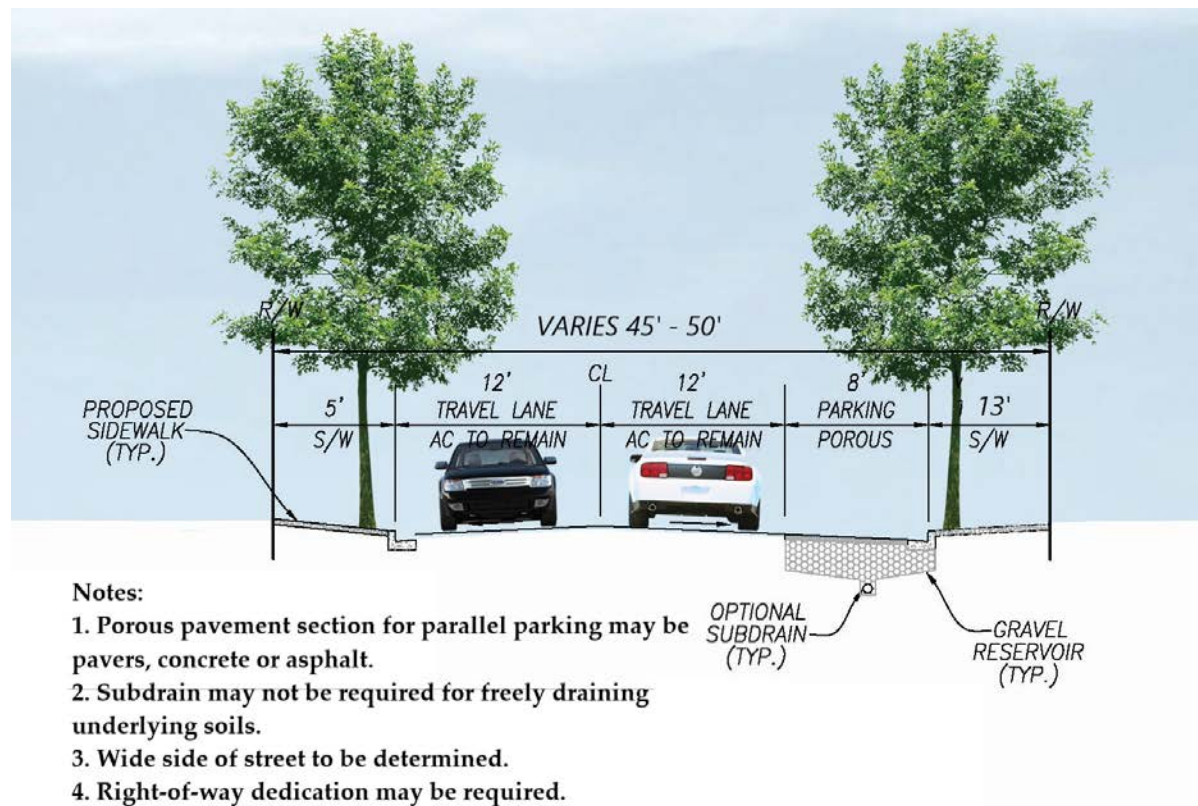


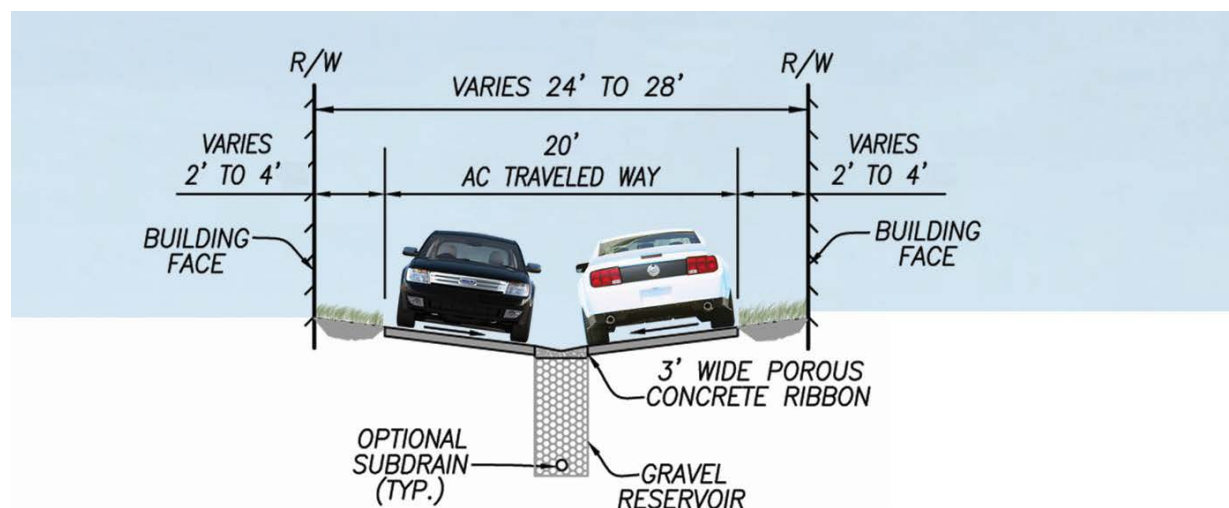
Figure 4-18 Peck Street, East of Main Street, Prospect Street and Olive Street
Cross-Section



4.3.3.10 Alleyways

An alleyway is a narrow lane found within the downtown, which usually runs between or behind buildings. Alleyways are designed to provide a vehicular service access at the rear of the properties for deliveries, fire access and parking. Some existing alleyways in downtown are very narrow at 15' wide. The Specific Plan proposes several 28' wide alleyways to provide a rear access to parking and garages. All alleyways within the downtown should be paved to provide a shared pedestrian and vehicular access.

Figure 4-19 Alleyways Cross-Section



Note:

Remainder areas typically landscaped with the exception of areas for building access (e.g. main door sidewalks and garage driveways)

4.3.4 Truck Routes

In the early 1990's, the City Council designated roadways as truck routes for the proper control of commercial truck traffic within the City. The establishment of truck routes directs commercial trucks exceeding a maximum gross weight of three tons to designated streets where they would cause the least amount of damage to pavement and where there is minimum intrusion and adverse impacts on residential areas and non-truck route streets.

The designation of truck routes does not prevent commercial trucks from using other roads or streets where necessary for the purpose of making pickups or delivery of goods, wares and merchandise from or to any building or structure located on restricted streets, or for the purpose of delivering materials to be used in repair, alteration, remodeling or construction of any building or structure upon the restricted street for which a building permit has been previously obtained. Figure 4-20 illustrates truck routes within the Downtown Elsinore Specific Plan area.



4.4 NON-VEHICULAR CIRCULATION

4.4.1 Bicycle Network

A primary and secondary bicycle trail system is to be incorporated into the Downtown Elsinore Specific Plan, which will provide connectivity throughout the specific plan and to other parts of the City of Lake Elsinore as part of the larger master plan for the City of Lake Elsinore. A primary bicycle trail is defined as a paved, accessible, multi-use path providing non-vehicular transportation corridors connecting each neighborhood and Planning Area to one another throughout the community.

Primary bikeways are divided into three classes: Class I, Class II, and Class IV. A Class I Bikeway path has its own, completely separate right-of-way for the exclusive use of bicycles only. A Class II Bikeway provides a restricted right-of-way for the exclusive or semi-exclusive use of bicycles with the permitting of vehicle parking and vehicle/pedestrian cross flows. A Class IV Bikeway is a bikeway for the exclusive use of bicycles and includes a required separation between the bikeway and the through vehicular traffic.

Secondary bicycle paths are also connective corridors, but are defined as secondary as they service, and are located throughout the local residential community, ultimately connecting to the primary bicycle trail system for total connectivity to the community at large. Within the Downtown Elsinore Specific Plan, secondary bicycle paths will generally be Class III bikeways. A Class III bikeway shares streets with motor vehicles or sidewalks with pedestrians.

Please refer to Figure 4-21 for a depiction of the Bikeways Plan.



**FIGURE 4-21
BIKEWAYS PLAN**



Prepared by:
City of Lake Elsinore GIS
August 21, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

4.4.1.1 Bikeway Standards

1. A Class I bikeway is a completely separated right-of-way for the exclusive use of bicycles. The right-of-way for Class I Bikeways may be substantial, and separated from roadways by landscaped strips or other barriers.

There are no specific Class I bikeways designated in the Downtown Elsinore Specific Plan, however, individual implementing development proposals will have the opportunity to incorporate Class I bikeways into their design, where appropriate. Class I bikeways must be paved and be a minimum 8 feet in width to allow two-way bicycle traffic.

Figure 4-22, below illustrates the typical cross-section of a Class I bikeway within the Downtown Elsinore Specific Plan:

Figure 4-22 Class I Bikeway



2. A Class II bikeway is a separate painted lane within the roadway adjacent to the curb of a vehicular roadway, and must be paved and be a minimum six feet wide. Class II bikeways in the Downtown Elsinore Specific Plan are proposed to be located at:

- ❖ Pottery Street
- ❖ Sumner Avenue, between the Outlet Channel and Main Street
- ❖ Limited Avenue
- ❖ Main Street, between Limited Avenue and Lakeshore Drive

Figure 4-23, below illustrates the typical cross-section of a dedicated Class II bikeway within the Downtown Elsinore Specific Plan and Figure 4-24 illustrates a typical cross-section for a shared Class II bikeway:

Figure 4-23 Class II Bikeway - Dedicated

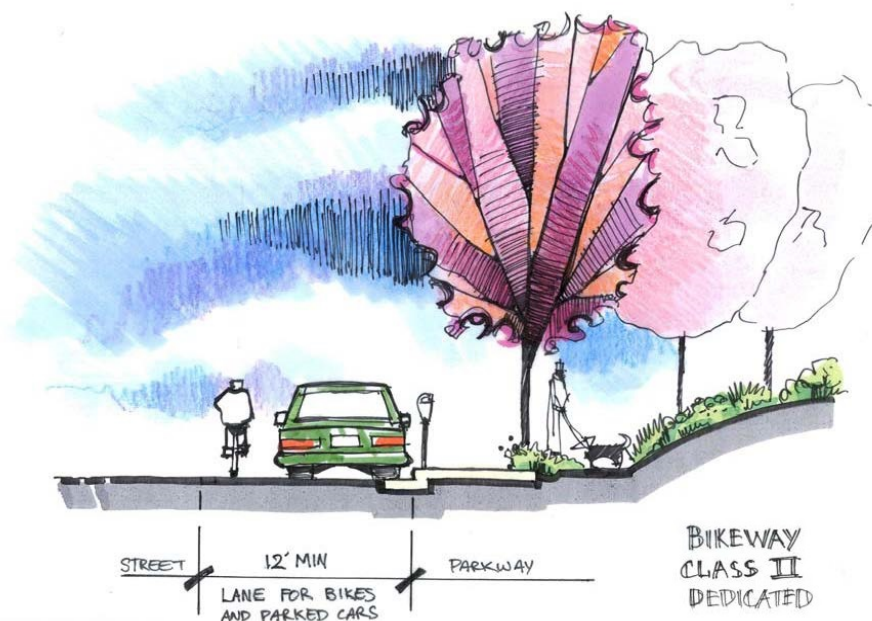
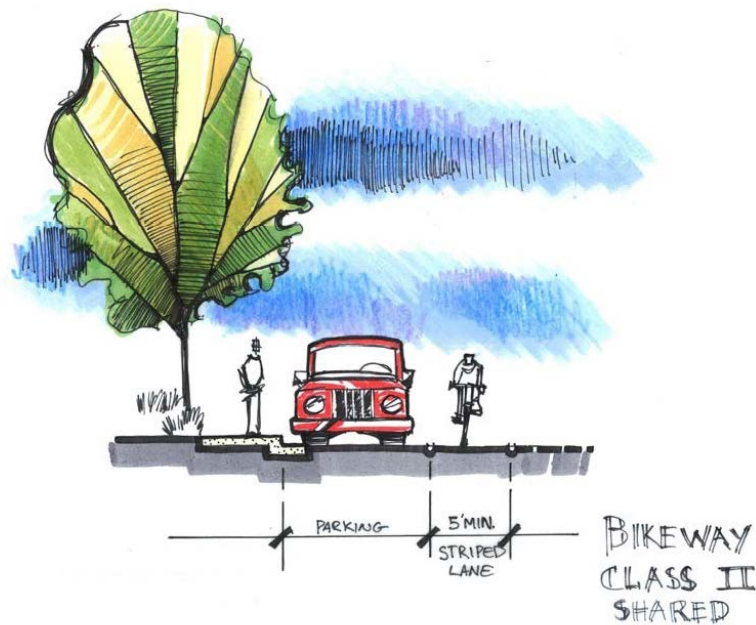


Figure 4-24 Class II Bikeway - Shared

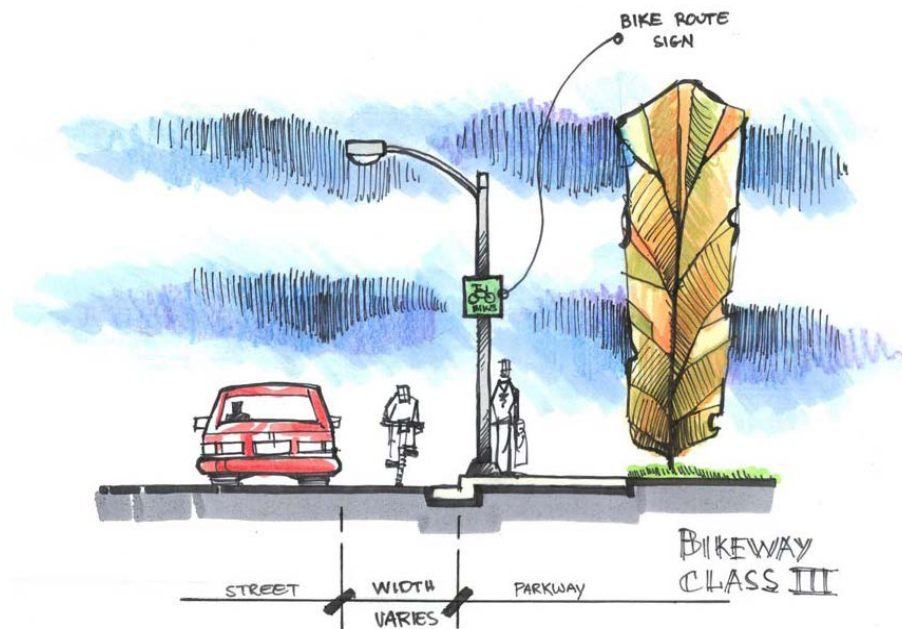


3. Class III bikeways (Bike Route) are shared facilities and are indicated only with bike route signs. These bikeways share the streets with motor vehicles or share the sidewalk with pedestrians. (Bicycle use is the secondary function in both conditions.) There is no minimum width for Class III bikeways as width is dependent upon many factors including volume and character of vehicular traffic on roads, typical speeds, vertical and horizontal alignment, sight distance and parking conditions. Class III bikeways in the Downtown Elsinore Specific Plan are proposed to be located at:

- ❖ Main Street, between Flint Street and Limited Avenue
- ❖ Graham Avenue, between the Outlet Channel and Chestnut Street

Figure 4-25, below illustrates the typical cross-section of a Class III bikeway within the Downtown Elsinore Specific Plan.

Figure 4-25 Class III Bikeway

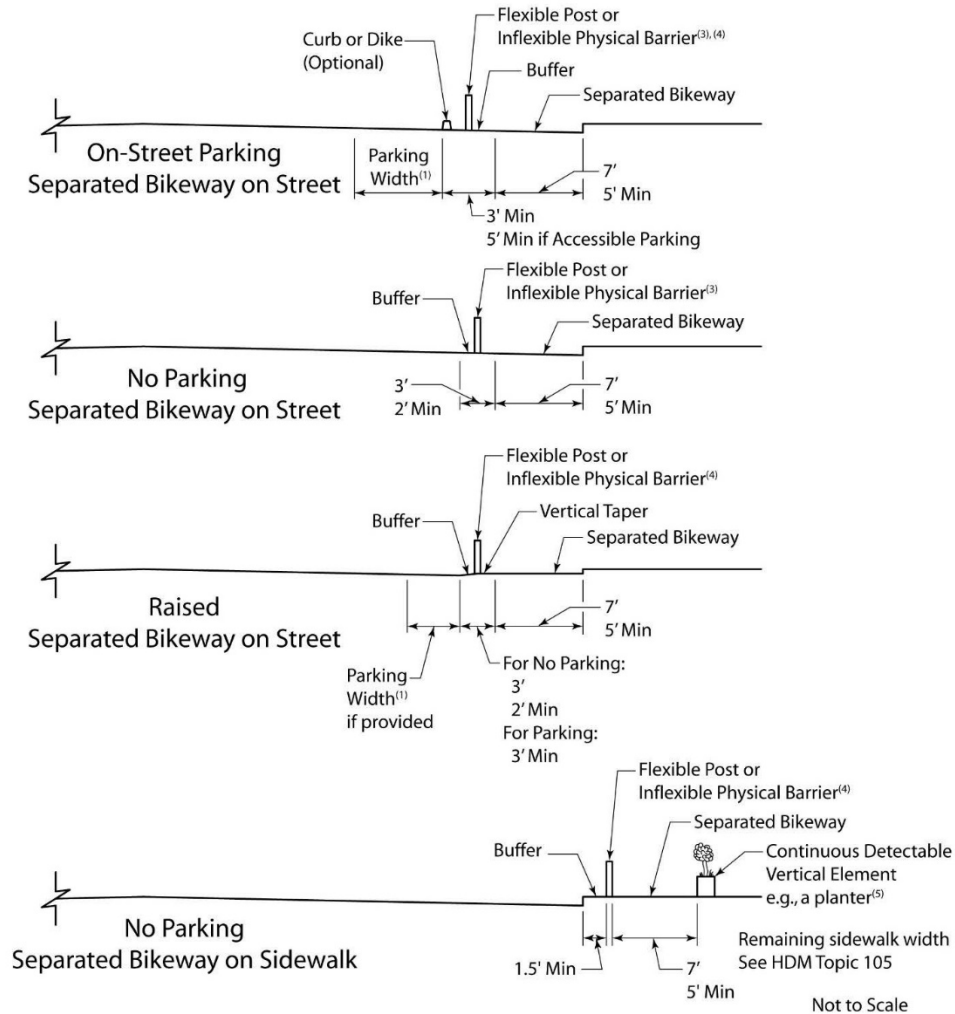


4. A Class IV Bikeway (separated bikeway) is a bikeway for the exclusive use of bicycles and includes a required separation between the bikeway and the through vehicular traffic. The separation may include, but is not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking. Separated bikeways typically operate as one-way bikeway facilities in the same direction as vehicular traffic on the same side of the roadway. However, two-way separated bikeways can also be used, usually in lower speed (35 miles per hour or less) environments.

Figure 4-26, below illustrates the typical cross-sections of Class IV bikeways within the Downtown Elsinore Specific Plan

Figure 4-26 Class IV Bikeways

Typical Class IV Bikeway (Separated Bikeway) Cross Sections



NOTES:

- (1) See CA MUTCD Section 3B.19.
- (2) For separated bikeway marking guidance, use the bicycle lane symbol marking per CA MUTCD Figure 9C-3 Option A.
- (3) May be a raised island in lieu of flexible posts or inflexible physical barriers.
- (4) Flexible posts or inflexible physical barriers may be omitted.
- (5) Periodic openings should be provided for bicyclists to access buildings.

Source: CALTRANS Design Information
Bulletin No. 89 (Class IV Bikeway Guidance)

5. All bikeways should be installed with rider safety as a primary concern, especially the use of proper lighting. Rider comfort should also be considered, such as rest stops with street furniture, including trash bins and benches.

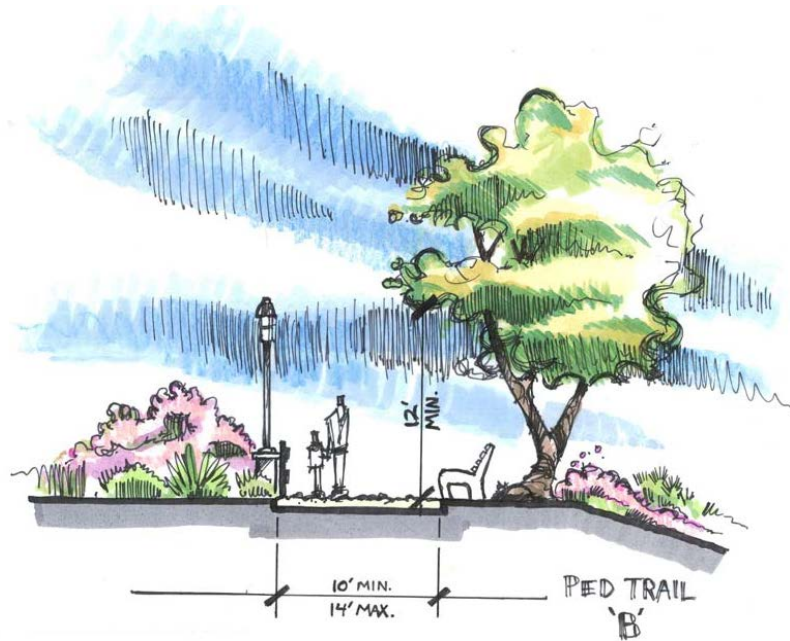
4.4.2 Off-Road Trails

The off-road trails complement the overall circulation system by providing alternative routes for pedestrians and bicyclists. This interconnected system of trails throughout the community promotes pedestrian, and bicycle activity between the various land uses, facilities and neighborhoods within the City and Downtown. In addition, the trails within Downtown are connected to Riverside County's trail system, providing an integrated regional trail network for hiking use. Please refer to Figure 4-28 for a depiction of the Trails Plan. The off-road trails provide both functional and recreational benefits to the community.

Among the many recreational benefits is the complete separation of the off-road trails from the vehicular circulation system providing a quiet environment in which to walk, jog, or bicycle without the intrusion of automobile traffic and noise. It also provides for the social interaction of community residents. Figure 4-27 shows an example of a typical greenbelt path cross-section.

Among the functional benefits is the reduction in the dependence on the automobile for transportation needs. As a result, traffic congestion is reduced and impacts to air quality are minimized.

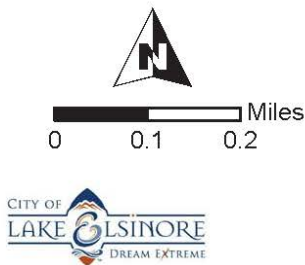
Figure 4-27 Type B Pedestrian Trail (Greenbelt Path)





NOTE: This figure identifies general locations and classifications of existing and proposed trails. Precise alignment and improvements shall be determined through the City's Design Review Process.

**FIGURE 4-28
TRAILS PLAN**



Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

4.4.3 Mass Transit

In addition to automobiles, bicycles, and pedestrians, the circulation system of Downtown encourages the use of mass transit as an alternative form of transportation. The Riverside Transit Agency covers 2,500 square miles of western Riverside County, and offers local fixed-route services that connect local communities, as well as CommuterLink express bus routes for long distance commuters traveling to Metrolink, Coaster and Sprinter stations, business parks, shopping malls and regional transit facilities. The RTA also partners with other agencies to provide 211VetLink, a premier source of travel information for veterans on the go.

RTA currently serves the Downtown area with a loop route. To minimize disruption to traffic flow, the street rights-of-way and adjacent landscape setback areas are sufficient to accommodate bus turnouts where warranted. Bus stop shelters and furniture are encouraged.

4.5 PARKING

The Downtown Vision seeks to create a safe, vibrant and walkable downtown centered along Main Street, that encourages a “park once and walk” setting. This methodology includes off-street parking, appropriately located consolidated parking lots or structures, subterranean-designed parking, alley and side-street access, alley-loaded parking structures, and structures behind buildings.

The City looks to add to its City-owned parking inventory in order to encourage further development/investment and to partner with the development community to seek creative solutions while supplying ample parking for all land uses in the Specific Plan. These solutions may include the formation of a Parking Implementation District, implementation of a Downtown Parking Management Program and/or paid parking.

4.5.1 Space Requirements

The Downtown Elsinore Specific Plan reduces the amount of parking required by the Lake Elsinore Municipal Code in order to encourage the “park-once-and-walk” objective. The number of parking spaces required for each land use within the Specific Plan set forth in Table 4-1.

Table 4-1 Downtown Elsinore Specific Plan Parking Requirements

Land Use	Parking Requirement
Commercial	1 space per 300 s.f.
Office	1 space per 333 s.f.
Civic	To be determined by Parking Analysis
Hotel	1 space per room
Single-family Residential	2 spaces per unit
Multi-family Residential	2 per unit plus 1 per 5 units for guest parking
Mixed Use	Sum of Commercial and Residential parking requirements
NOTES: ¹ The term square foot (s.f.) means the gross square footage of the floor area. ² The term "unit" means dwelling unit (d.u.).	

A developer may propose deviations from the parking requirement. Such request must be supported by a parking study prepared by a licensed engineer and will be subject to review and approval by the City.

Shared parking and off-street parking will be considered for purposes of serving minimum parking requirements on a case-by-case basis.

Existing on-site parking supply will be considered full satisfaction of the minimum parking requirement for new land uses that occupy existing buildings.

Where this Specific Plan is silent, parking requirements set forth in LEMC Chapter 17.148 will apply.

4.5.2 Public Parking

Intent: *To promote a safe and pleasant pedestrian environment throughout the Downtown Elsinore Specific Plan; creating parking lots and structures that supply an adequate number of parking spaces for existing and new developments and that are located at a walkable distance of commercial uses and cultural activities.*



❖ New surface parking lots should be located internally in the city block, surrounded by buildings.

❖ Existing parking lots that are located adjacent to the street should be properly screened from views from the sidewalk and street by a hedge or allow masonry wall and vines.

❖ A minimum three- foot high visual screen must be provided when parking areas are adjacent to neighboring uses. Screening may consist of trees, shrubs, vines, ground cover, berms, walls, or a combination thereof.

❖ Where parking areas are located adjacent to residential uses, a six-foot high screen wall or fence is required. Walls and fences must follow the standards and guidelines in Section 3.6.3 (Architectural Character).

❖ Vehicular access to parking lots or structures should be located to minimize impacts to the traffic flow in the public right of way, minimize the number of curb cuts and secure pedestrian and vehicular safety.

❖ Shared parking should be encouraged and total number of parking spaces required could be reduced if a shared parking analysis reveals a lower accurately derived parking demand.

❖ In the design of an above grade parking structure, avoid sloping floors visible from adjacent streets and buildings.

❖ Parking structures should be located preferably along service streets with convenient regional accessibility, without adding or interfering with internal vehicular circulation in the Downtown area.

❖ Façade treatment of parking decks should incorporate landscaping and other architectural elements to minimize visual impact.



- ❖ Whenever feasible, ground floor retail should be incorporated into the parking structure.
- ❖ Underground parking, parking structures, or parking areas located behind the development project, away from Main Street, are encouraged.

4.5.3 Parking Development Standards

1. Driveway access points should be located as far as possible from street intersections.
2. Care should be taken to ensure that urban design concepts such as linear plazas and visual corridors are not compromised by driveways.
3. Site access and circulation should promote pedestrian and vehicular traffic safety and convenience.
4. A continuous circulation network should be provided throughout the site to the greatest extent possible.
5. Vehicle maneuvering, stacking, and emergency access must be provided on site.
6. Pedestrian walks should be fully integrated with the internal site vehicular circulation system to provide safe, walkable and landscaped pedestrian access through parking areas to building entrances.
7. Parking lot design and walkways should minimize use of impervious surfaces in a manner consistent with NPDES requirements.



4.5.4 Bicycle Parking

1. Bicycle parking should be provided when a new building is constructed, an existing building is enlarged or otherwise structurally altered, or the use or occupancy is intensified, expanded or changed.
2. The determination of whether bicycle-parking facilities (in the form of both racks and lockers) are required shall be determined as follows:
 - a. Residential. Projects of five or more total dwellings shall provide bicycle parking

facilities equal to or greater than 10% of required automobile parking.

- b. Commercial. All commercial projects shall provide bicycle parking facilities equal to or greater than 10% of required automobile parking.
 - c. Mixed Use Projects. The total requirement for bicycle parking facilities on a site shall be the sum of the requirements for each of the uses occupying the site.
 - d. Uses not specified. The decision-making for uses not set forth herein. Such determination shall be based upon the requirements for the, most comparable uses specified in this section.
 - e. Where the total number of bicycle parking facilities required includes a fraction of a facility, one additional bicycle facility shall be added for each such fraction.
3. Motor vehicle entrances must display adequate signs to indicate the availability and location of the bicycle parking facilities.
4. The following guidelines will apply:
- a. Bicycle parking may be provided in sheltered, secured facilities located on the project site, in conjunction with off-street parking, or in bike racks on the public sidewalk provided full ADA-compliance pedestrian access is maintained.
 - b. Bike racks shall be artistically designed and shall be designed so that rack use is intuitive. (i.e., First-time users should recognize the rack as bicycle parking and should be able to use it without the need for written instructions.)
 - c. Exact locations and designs shall be subject to the approval of the Community Development Director.

5 INFRASTRUCTURE

5.1 INTRODUCTION

The Downtown Elsinore Specific Plan anticipates that the Downtown will experience future commercial, mixed use and residential growth and development. Public buildings and a network of water, sewer and storm water drain facilities are necessary to support existing and new residents and businesses. This chapter describes the infrastructure that serves the Downtown and includes policies to ensure a high quality environment in the Downtown.

5.2 PUBLIC BUILDINGS

Public buildings are essential components of a healthy, strong and vibrant downtown. Existing and future public buildings in the downtown area such as City Hall, the Cultural Center, a future library and a new post office will be encouraged and will draw many employees and users of public services who are likely to spend time and money at downtown businesses.

5.3 WATER AND SEWER

5.3.1 Introduction

The Elsinore Valley Municipal Water District (EVMWD) is a public nonprofit agency that was created on December 23, 1950, under the Municipal Water District Act of 1911. EVMWD provides water, wastewater, and reclaimed water service to the City of Lake Elsinore, the cities of Canyon Lake and Wildomar, portions of the city of Murrieta, and unincorporated portions of Riverside County. EVMWD is a special district, whose powers include provision of public water service, water supply development and planning, wastewater treatment and disposal, and recycling. Currently, EVMWD has more than 35,000 water, wastewater, and agricultural service connections. EVMWD is a subagency of the Western Municipal Water District, a member agency of the Metropolitan Water District of Southern California (Metropolitan). The District's approximately 96 square mile service area includes the Downtown Elsinore Specific Plan and Lake Elsinore.

5.3.2 Water Supply

EVMWD obtains its potable water supplies from imported water from Metropolitan, local surface water from Canyon Lake, and local groundwater from the Elsinore Basin. It has access to groundwater from Elsinore Basin, Coldwater Basin, San Bernardino Bunker Hill Basin, Rialto- Colton- and Riverside-North Basin. Almost all of the groundwater production that is used for potable use occurs in the Elsinore Basin. Imported water supply is purchased from the Metropolitan via Eastern Municipal Water District and Western Municipal Water District.

EVMWD's service area is broken into two divisions, the Elsinore Division and the Temescal Division (also known as Temescal Domestic Service Area). The division between the two valleys is approximately two miles north of Lake Elsinore, near the intersection of Love Lane and Temescal Canyon Road (where the Temescal Wash flows north). The water system currently includes 41 pressure zones. Within these zones, there are approximately 3,618,000 feet (685 miles) of pipelines ranging in diameter from 4 inches to 42 inches, 73 storage reservoirs with an approximate total storage capacity of 93 million gallons (MG) and 51 booster pump stations. The District currently obtains its water from 12 groundwater wells, the Canyon Lake Water Treatment Plant (WTP), and imported water from Metropolitan through the Auld Valley and Temescal Valley Pipelines.

EVMWD's existing recycled water demands are supplied by tertiary-treated wastewater from the Regional Water Reclamation Facility (WRF), Railroad Canyon WRF, and Horsethief WRF. In the effort to minimize the need for imported water, EVMWD plans to expand its recycled water system to provide recycled water for irrigation users and to maintain water levels in Lake Elsinore during normal and dry years.

The 2016 Water System Master Plan dated August 2016 was prepared for EVMWD. The District's consultant, MWH, also prepared EVMWD's 2016 Recycled Water System Master Plan dated August 2016.

The Downtown Elsinore Specific Plan is located within EVMWD's 1579 Pressure Zone. Existing water mains are located throughout the Downtown area.

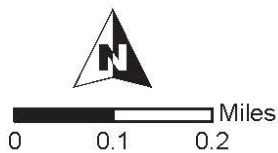


FIGURE 5-1
EVMWD WATER MAINS

Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

5.4 WASTEWATER

The EVMWD Sewer District provides service for the City of Lake Elsinore, the cities of Canyon Lake and Wildomar, portions of the city of Murrieta, and unincorporated portions of Riverside County. The “backbone” of the system consists of trunk sewers, generally 10 inches in diameter and larger, that convey the collected wastewater to EVMWD’s Water Reclamation Facilities (WRFs). EVMWD’s existing wastewater collection systems consist of approximately 406 miles of pipe (force mains and gravity sewers) up to 54 inches in diameter, 38 active lift stations and three WRFs.

EVMWD’s current service area is delineated into four separate collection systems (sewersheds). These are the Regional WRF, Railroad Canyon WRF, Horsethief WRF, and Southern collection systems. EVMWD’s Regional, Railroad Canyon, and Horsethief WRFs treat the flows conveyed in the Regional, Railroad Canyon, and Horsethief collection systems, respectively. Whereas wastewater discharged into the Southern collection system is conveyed through the Rancho California Water District’s (RCWD’s) wastewater collection system to the RCWD operated Santa Rosa WRF for treatment.

The 2016 Sewer System Master Plan Final Report dated August 2016 was prepared for EVMWD by MWH. The EVMWD Sewer System Master Plan makes recommendations for improvements, such as gravity sewer mains, force mains, lift stations, and wastewater treatment facilities.

EVMWD also produces recycled water. Recycled water is used to irrigate parks, street medians, golf courses, and wildlife habitat and to provide lake stabilization. It is the goal of EVMWD to build additional lines and expand recycled water services in order to free up water for additional residential uses.

The Downtown Elsinore Specific Plan area is within the Regional WRF sewershed. Existing sewer mains are located throughout the Downtown area.

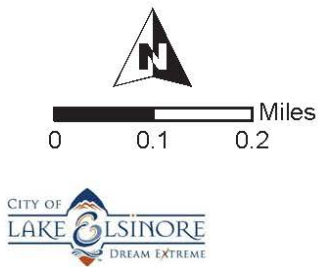


FIGURE 5-2
EVMWD SEWER MAINS

Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

5.4.1 Water and Sewer Development Standards

1. All water and sewer lines shall be placed underground and shall comply with Elsinore Valley Municipal Water District (EVMWD) standards.
2. All water and sewer lines shall be maintained by EVMWD.
3. All water and sewer lines shall be adequately sized to service potential future growth, where appropriate.
4. The costs associated with the construction of over-sized lines to serve existing or future off-site developments shall be subject to reimbursement consistent with EVMWD's regulations.
5. Any design of off-site facilities, if required, shall be coordinated with the affected property owner(s) and EVMWD.
6. The design of all proposed water facilities shall provide sufficient fire protection as determined by the City of Lake Elsinore Fire Marshal.

5.5 STORM WATER DRAINAGE

5.5.1 Conceptual Master Drainage Plan

The drainage plan for the Downtown Elsinore Specific Plan is part of the Lake Zone portion of the City's Draft Master Drainage Plan (MDP). The primary purpose of updating the MDP is to provide a comprehensive storm water strategy that provides flood protection for both existing and future developments within the City. It is designed to safely and efficiently convey storm water throughout the project area, while protecting the health, safety, and welfare of the general public. It is a significant component in the solution to regional drainage issues, and contributes to the ongoing implementation of the Lake Elsinore Management Project.

A master drainage plan addresses the current and future drainage needs of a given community. The proposed facilities may include channels, storm drains, levees, basins, dams, wetlands or any other conveyance capable of economically relieving flooding problems within the plan area. The plan includes an estimate of facility capacity, sizes

and costs.

The alignment and location of the facilities proposed in the Draft MDP update, as shown in Figure 5-3 and described below, are conceptual. Precise locations and size of the facility will be dictated by site-specific conditions and other factors such as environmental, engineering and economic considerations.

Line L-4

Line L-4 is a 36" single line storm drain, which begins at the intersection of Langstaff Street and Pottery Street, travels along Pottery to the east for a distance of 420 feet, and outlets to an existing 24" stub out on Outlet Channel.

Line L-5

Line L-5 is a 36" single line storm drain, which begins at Langstaff Street and terminates at Flint Street, west of Outlet Channel, outlets to an existing stub out of the channel.

Line L-6 Storm Drain System

Line L-6 storm drain system consists of a mainline storm drain and an existing 54" Caltrans culvert and 600' feet of Caltrans concrete lined drainage ditch. The upstream origin of Line L-6 begins approximately 520' north of the existing 54" Caltrans culvert, along Elsinore Hill Road proposed by Tentative Tract Map 35337, Spyglass Ranch, as a 36" RCP, and discharges into the existing 54" pipe culvert south of the I-15 Freeway. Line 6 reassumes its alignment at the end of Caltrans ditch near the north end of Lookout Street as a 42" RCP. From there the 42" RCP extends 330 feet southerly on Lookout Street, turns westerly along Flint Street as a 48" RCP for 340 feet, increases its size to a 54" RCP for approximately 1,240 feet, then discharges into the Outlet Channel.

Line L-7 Storm Drain System

Similar to the Line L-6 system, Line L-7 storm drain system consists of a mainline storm drain and three existing Caltrans culverts and a section of natural drainage ditch along the south side of the I-15 Freeway. The upstream origin of Line L-7 begins approximately 1070' north of the existing 39" Caltrans culvert, along a natural valley as a 36" RCP, and discharges into the existing 39" pipe culvert north of the I-15 Freeway. Line 7 reassumes its alignment near the north end of Adobe Street as a 42" RCP. From there the 42" RCP extends 790 feet southerly along Adobe Street, turns westerly along Pottery Street as a

54" RCP for 1410 feet. At the intersection of Pottery Street and Ellis Street, Line L-7 increases its size to a 60" RCP along Pottery Street for a distance of approximately 1,070 feet and discharges into the Outlet Channel.

Line L-8 and L-9 Storm Drain Systems

Line L-8 and L-9 systems are located east of the District's Outlet Channel. This area comprises mostly older residential neighborhoods with approximately 33 acres of steep hillside at its easterly end. Currently there is no existing storm drain facility for this area. Our field investigation indicated that this area experiences some street flooding, ponding and hillside erosion at various locations.

Line L-8 is a single line 36" RCP storm drain totaling 650 feet in length, which begins at Main Street, extends westerly along Sumner Avenue, and outlets to the Outlet Channel.

Line L-9 system is a single line 36" RCP storm drain totaling 600 feet in length, which begins at Main Street, extends westerly along Franklin Street, and outlets to the Outlet Channel.

5.5.2 Water Quality

In addition, the Draft MDP will also explore the opportunity for implementing watershed-based Best Management Practices (BMPs), and water quality mitigation and treatment with the focus on nutrient reduction, to enhance the beneficial use of, and minimize the negative impact to the Lake water quality from current and future development. The 1987 Clean Water Act sets forth design requirements to ensure that the optimum water quality from urban runoff is achieved. These requirements must be adhered to in order to comply with National Pollutant Discharge Elimination System (NPDES) permits. An NPDES permit requires that specific BMPs be implemented both during and post construction. The BMPs that new development and significant redevelopment must implement are required to treat and release runoff to provide for recharge of the Lake. Low Impact Development (LID) BMPs such as bioretention, vegetated swales, detention basins, and wet ponds, utilize biofiltration/biological processes to remove pollutants, reduce hydromodification impacts and provide sediment control. Proprietary BMPs that rely on mechanical means and filters can also be used as long as they effectively treat the pollutants of concern prior to discharge into the Lake and are approved by the Regional Water Quality Control Board. Proprietary devices include catch basin filter inserts and hydrodynamic separators, and filter based modular wetlands.

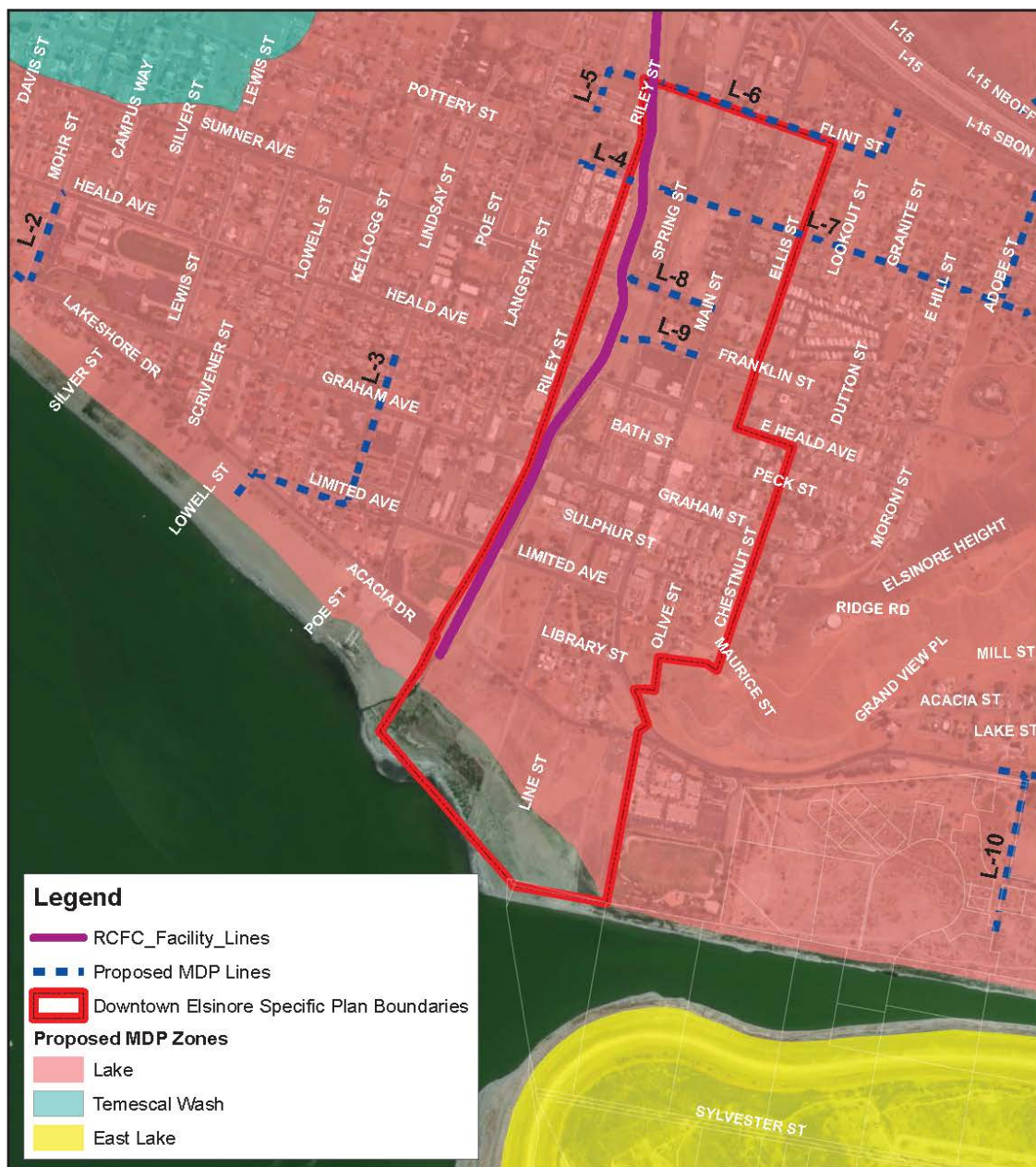


FIGURE 5-3
CONCEPTUAL DRAINAGE PLAN



Prepared by:
City of Lake Elsinore GIS
July 17, 2018
Data Sources:
County of Riverside GIS
City of Lake Elsinore GIS
Stateplane NAD 83

5.5.3 Drainage Development Standards

1. The Draft Master Drainage Plan indicates the approximate location and sizing of drainage lines and facilities within the Downtown Elsinore Specific Plan. However, precise system layout and sizing shall be determined during tract map preparation, or at the earliest possible project design.
2. Drainage improvements are designed and constructed in compliance with the RCFCWCD Hydrology Manual and USACE Permit 2004-00748-RRS.
3. All drainage facilities in this project larger than 36" shall be submitted for review and approval for maintenance to Riverside County Flood Control District.
4. Flood flow criteria to be used for the design of waterways, channels and closed conduits shall have the following minimum average recurrence intervals:
 - a. The design of storm drain systems shall be based on a minimum 10-year frequency.
 - b. For sump conditions, a 25-year frequency shall be applied, and a connected system shall be designed to completely convey 100-year runoff, with a secondary outlet to ensure the water surface is at least one foot below the proposed pad elevations.
5. On-site drainage facilities that are located outside of street right-of-ways shall be contained in drainage easements.
6. Streets shall be oriented to provide maximum potential conveyance of regional flooding during significant storms in order to expedite the flow of storm-water throughout the Downtown Elsinore Specific Plan.
7. Any development shall complete and submit for review and approval to the Engineering Division both a preliminary and final Water Quality Management Plan (WQMP), incorporating LID Principles and Stormwater Best Management Practices (BMPs).
 - a. The Preliminary WQMP shall be approved prior to scheduling for public hearings, and the Final WQMP shall be approved prior to issuance of any grading or building permit.
 - b. Water quality facilities that service more than one parcel shall be placed in an easement to provide for maintenance and to prevent obstruction.

- c. Parking lot landscaping shall be designed with concave landscape grading, and provide for treatment, retention or infiltration of runoff.
 - d. Project hardscape areas onsite shall be designed and constructed to provide for drainage into adjacent landscape and permeable surfaces in low traffic roads and parking lots.
 - e. Project road right-of-way shall be designed and constructed to provide for treatment of associated runoff.
 - f. Multi-family, commercial, industrial and high-density residential and public parks shall be designed to include full capture trash devices as approved by the State and/or Regional Water Quality Control Board.
 - g. The project shall identify potential Hydraulic Conditions of Concern (HCOC) and implement measures to limit disturbance of natural water bodies and drainage systems, conserve natural areas, protect slopes and channels and minimize significant impacts from urban runoff.
 - h. Prior to the issuance of any grading or building permit, the developer shall submit for review and approval by the City Engineer, an Erosion and Sediment Control Plan, to demonstrate compliance with the City's National Pollutant Discharge Elimination System Program, California Building Code, and state water quality regulations for grading and construction activities. The Plan shall identify how all construction materials, wastes, grading or demolition debris, and stockpiles of soil, aggregates, soil amendments, etc., shall be properly covered, stored and secured to prevent transport into local drainages or waters by wind, rain, tracking, or dispersion. The Plan shall also describe how the project will ensure that all BMPs will be maintained during construction of any future right of ways.
 - i. After construction, the developer shall record an Operation and Management Plan (O&M) that (1) describes the long-term operating and maintenance requirements for BMPs identified; (2) identifies the entity that will be responsible for long-term operation and maintenance of the referenced BMPs; (3) describes the mechanism for funding the long-term operation and maintenance of the referenced BMPs; and (4) provides for annual certification of water quality facilities by a registered civil engineer and/or the City for a fee if the service is available.
8. BMPs shall be implemented to enhance pollutant removal during storms, and to improve the quality of storm-water runoff. BMPs refer to non-structural or structural pollution control measures that reduce the amount of non-point source pollution

entering the Waters of the United States. Non-structural BMPs are those in which pollutants are kept from coming in contact with storm-water. Structural BMPs are those in which various methods of treating storm-water are used.

9. The following non-structural BMPs are recommended:
 - a. Routine inspection of catch basins before and after the storm seasons. Routine inspection shall include cleaning, where necessary, to remove sediment and debris containing absorbed pollutants.
 - b. Guidelines for the use of fertilizers and pesticides, including proper application rates for the local area and soil type.
 - c. Erosion control measures to minimize the effects of erosion on open slopes adjacent to development areas. These measures shall be considered part of all required grading permit approvals.
 - d. The maintenance of non-structural BMPs shall be the responsibility of the various property owners or business owners within the downtown area.
 - e. Good housekeeping practices shall be recommended that include sweeping up yard waste, timely cleanup of oil and/or chemical spills using dry methods, proper pool and spa maintenance, eliminating landscape irrigation overspray, etc.
 - f. Cleanup and proper disposal of pet waste shall be recommended.
10. Structural BMPs, when required, shall consist of one or more of the following measures depending upon site conditions and the ultimate discharge requirements:
 - a. Vegetated swales and bio-filters
 - b. Water quality ponds or detention basins for “first flush” runoff.
 - c. Water quality inlets
 - d. Porous pavement and/or pavers
 - e. Full capture of trash.
 - f. Proprietary devices

6 PUBLIC SERVICES

6.1 INTRODUCTION

The Downtown Elsinore Specific Plan anticipates that the Downtown will experience future commercial, mixed use and residential growth and development. Public services provided by the City and other agencies will support existing and new residents and businesses. This chapter describes the public services that serves the Downtown and includes policies that will be applied in the Downtown.

6.2 POLICE SERVICES

The City of Lake Elsinore contracts for police protection with the Riverside County Sheriff's Department. The Sheriff's Department operates in Lake Elsinore as the Lake Elsinore Police Department. The Sheriff's Department has mutual aid agreements with all of the local law enforcement agencies within Riverside County. In addition, the Department coordinates with the State Office of Emergency Services to provide and



receive statewide mutual aid when necessary. The Lake Elsinore Sheriff's Station is located at 333 Limited Avenue, adjacent to the western boundary of the Project Site. The Lake Elsinore Sheriff's Station serves an area of 241 square miles, including the City of Lake Elsinore, the City of Wildomar, and the unincorporated communities of Alberhill, El Cariso, Glen Eden Hot Springs, Glen Ivy Hot Springs, Good Hope, Lakeland Village, Quail Valley, and Sedco Hills. This service area includes the Project Site.

Development within the Downtown Elsinore Specific Plan shall incorporate “defensible space” design concepts within each new development proposal. These defensible space design concepts include but are not limited to circulation for pedestrians, bicycles, vehicles, and police patrols, lighting of streets, walkways, parking areas and bikeways, visibility of doors and windows from the street and between buildings, and visual surveillance of open space areas. Crime prevention measures shall be also used during the individual site and building layout design.

6.3 LAKE PATROL

The Lake Elsinore Police Department manages the Lake Patrol with Marine Safety trained police officers who patrol the Lake, beaches, and lake-adjacent parks. Officers enforce boating rules and regulations on the Lake and assist stranded boaters. The Lake Patrol is augmented by a special group of volunteers known as Lake

Elsinore Marine Search and Rescue (LEMSAR) who also patrol the Lake and assist with boating collisions and stranded vessels. LEMSAR volunteers are trained in first aid and CPR and must complete a U. S. Coast Guard auxiliary boating and safety course.



6.4 FIRE SERVICES

The City of Lake Elsinore contracts for fire services with the Riverside County Fire Department (RCFD) and the California Department of Forestry and Fire Protection (CAL FIRE). The County of Riverside contracts with the State of California for fire protection. Public Resources Code (PRC) Section 4142 affords legal authority for CAL FIRE to enter into cooperative agreements with local government entities to provide fire protection services with the approval of the Department of General Services. All of the Riverside County fire stations are part of the Integrated Fire Protection System, under contract with the State. All calls for service are dispatched by the same County Fire 9-1-1 Center.



Battalion 2 in the Southwest Division of RCFD services the City. In addition to emergency and fire services, RCFD provides administration, personnel, finance, dispatch, fire prevention, hazardous materials control, training, emergency services, and arson investigation. Fire paramedics are located at each station. The City's four fire stations are as follows:

- Fire Station No. 10 (Elsinore), servicing the central area of the city, located less than ¼ mile west of the Project boundaries at 410 W. Graham Ave. (Not funded as of July 1, 2017.)
- Fire Station No. 85 (McVicker Park), located to the north at McVicker Park, slightly east of the lake at 29405 Grand Avenue.
- Fire Station No. 94 (Canyon Hills), located in the southeast section of the City, at 22770 Railroad Canyon Road, east of the I-15.
- Fire Station No. 97 (Rosetta Canyon), located in the north section of the City on Rosetta Canyon Drive.

The following stations are located outside city limits:

- Lakeland Village Station No. 11.
- El Cariso Station No. 51.
- Canyon Lake Station No. 60
- Wildomar Station No. 61.
- Rancho Capistrano Station No. 74.

Although the fire stations are operated by RCFD, CAL FIRE staffs firefighters and stores firefighting equipment at stations throughout the City, particularly during peak fire season. Both agencies respond to all types of emergencies, depending on the need and equipment available. Emergencies range from wildland fires, residential/commercial structure fires, automobile accidents, medical aid requests of all types, search and rescue missions, hazardous material spills, floods, earthquakes, and more. Standard response times are established by RCFD guidelines. The response time goal is to arrive at any location within the City in seven minutes, with the intent to reduce that time to five minutes.

Since October 2003, fire paramedics are required at each station. These specially trained firefighters are equipped to respond to medical emergencies and ride on all calls. Their arrival on the scene can ensure the timely start of emergency medical treatment until an ambulance arrives for patient transport. Each fire engine carries nearly \$35,000 worth of state-of-the-art emergency medic equipment.

The City of Lake Elsinore Community Development Department has County Fire personnel in-house providing fire protection, inspection and plan-check services.

Developers in the Downtown area will be expected to provide for a proportional mitigation to fire service impacts via capital improvements and/or impact fees.

6.5 SCHOOLS

The Lake Elsinore Unified School District (LEUSD) serves the City of Lake Elsinore including the City's downtown area, the cities of Canyon Lake and Wildomar, and a portion of the unincorporated County of Riverside. The LEUSD covers a 144 square mile area with a population of approximately 100,000. LEUSD has 24 schools including 14 elementary, four middle, and three high schools, a continuation school, an alternative education center, and an adult education program.

The LEUSD's schools that would likely serve the project area include Elsinore Elementary School and Railroad Canyon Elementary School, Elsinore Middle School, Temescal Canyon High School and Elsinore High School. Depending on school capacity, these or other LEUSD schools will provide for the school needs of the Downtown Elsinore Specific Plan. As residential development occurs, the property owner or developer will pay school fees as required by LEUSD and the State of California.



6.6 SOLID WASTE AND RECYCLING

Currently, CR&R, Inc. provides solid waste collection and hauling services within the City under contract with the City of Lake Elsinore. CR&R is responsible for trash disposal in the City of Lake Elsinore as well as in Temecula, Canyon Lake, and parts of the unincorporated County of Riverside. Residents are provided a 60-gallon trash container for garbage. Trash is taken to either a landfill within Riverside County or the Materials Recovery Facility (MRF). There are no active landfills in the City. Riverside County Department of Waste Resources (RCDWR) manages the landfills used by the City of Lake Elsinore. Capacity levels of landfills within RCDWR's jurisdiction are calculated according to the system-wide capacity level. Landfills within their jurisdiction adhere to state guidelines, which specify that a minimum of 15 years of system-wide landfill

capacity shall be provided.

RCWMD facilitates waste management services for Riverside County. These services are provided on a countywide basis, and each private or public entity determines which landfill or transfer station to use. Typically, this determination is made based on geographic proximity. The landfills typically used by the City of Lake Elsinore are the El Sobrante, Badlands, and Lamb Canyon Landfills. All three of the landfills are Class III municipal solid waste landfills.

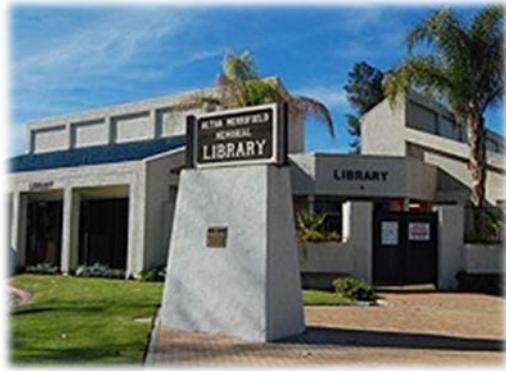
A household hazardous waste is any waste generated by households that can cause illness or death or pose a threat to health or the environment when improperly stored, disposed, or otherwise managed. Establishment of permanent collection centers or periodic collection events at temporary locations are the most common methods for gathering household hazardous waste for disposal other than through the municipal garbage collection system. Through ongoing cooperation between the City of Lake Elsinore and the Riverside County Waste Management District, the Lake Elsinore Regional Permanent Household Hazardous Waste Collection Facility (PHHWCF) serves City and County residents. The PHHWCF is located at 521 North Langstaff Street within the City of Lake Elsinore.

6.7 ENERGY

Southern California Edison and the Southern California Gas Company currently provide electrical power and natural gas respectively to the City of Lake Elsinore and the Project area.

6.8 LIBRARY SERVICES

The City of Lake Elsinore is part of the Riverside County Library System. Residents have access to 29 libraries and two bookmobiles. There are two libraries within city boundaries, including Lake Elsinore Library located at 600 West Graham Avenue, located approximately ¼ mile west of the Downtown Elsinore Specific Plan boundaries, and Lakeside Library on Riverside Drive, located northwest of the lake. The Canyon Lake Library is just outside the city boundary on Railroad Canyon Drive, and the Wildomar Library is just outside the city boundary at 34303 Mission Trail, Wildomar.



The branch located at 600 West Graham Avenue in the City provides library facilities in the project area. All branches of the county library system are supported by volunteer nonprofit "Friends of the Library" organizations. Dues, used book sales, rental books, videos and the sale of novelty items are the primary fundraising activities. Funds raised are used to support library programs and to supplement library resources.

Section 16.34.060 in Chapter 16.34 (Required Improvements) of the Lake Elsinore Municipal Code requires that prior to the issuance of a building permit, the applicant pay fees for the purposes set forth in that section. Paragraph B of Section 16.34.060 describes the City's Library Mitigation Fee and requires that "an in-lieu fee for future construction of library improvements shall be paid to the City of Lake Elsinore to assure the necessary library facilities are provided the community. Such facilities are to meet the Riverside City/County Library standards." The in-lieu fee is established by resolution and all residential developments, including apartments, condominiums, fourplexes, triplexes, duplexes, mobile homes, and single-family residences within the Downtown Elsinore Specific Plan will be required to pay the one-time library fee at the current rate in effect at the time of payment. The City of Lake Elsinore, in cooperation with the County Library System, strives to improve and expand library services in the Downtown Elsinore Specific Plan area.

6.9 ANIMAL SERVICES

The City contracts with a private company called Animal Friends of the Valleys (AFV) for all animal control services.

AFV also provides animal services to the cities of Canyon Lake, Wildomar, Murrieta and Temecula. AFV humane and animal services officers respond to calls from 8 a.m. to 5 p.m. Monday through Saturday, and respond to all emergencies to the above cities 24 hours per day. AFV is located at 33751 Mission Trail in Wildomar, and open to the public from 10 a.m. until 4 p.m. Monday through Saturday, with evening extended hours on Wednesdays until 7 p.m. The organization is dedicated to promoting humane care of animals through education and a proactive animal services program. The organization works to prevent animal suffering and to curb pet overpopulation.

6.10 TELECOMMUNICATIONS

Verizon provides the local landline telephone service, although long distance services may also be obtained from a number of other providers. In addition, a number of companies provide wireless or cell phone services. Time Warner Cable provides cable television and high-speed Internet.

6.11 MOSQUITO AND VECTOR CONTROL

The Northwest Mosquito and Vector Control District provides comprehensive vector control services to over 300 square miles and to more than 700,000 residents, including those within the City of Lake Elsinore. An eight-member Board of Trustees appointed by the respective city councils and Riverside County Board of Supervisors governs the District.

The District has a state-of-the-art laboratory staffed by trained scientists and researchers. The District conducts in-house research and collaborates with University of California and California State university scientists. The District library has the most recent scientific journals and books in stock. The District also provides public education and outreach to the community. The outreach program provides information to the residents about vectors and vector-borne diseases, how to reduce breeding sources on their property, and disseminates information regarding disease outbreaks and District activities.

Services provided by the District to the City include year-round surveillance of vector populations and vector-borne disease. The District also conducts regular routine inspections of all vector-breeding sites, and applies the most environmentally compatible materials to eliminate immature and adult vectors. The District staff utilizes the latest specialized control equipment to treat a wide range of breeding sources.

6.12 PUBLIC SERVICES DEVELOPMENT STANDARDS

1. All utility distribution lines shall be placed underground. Utility distribution lines shall be installed concurrently wherever feasible.
2. Where practicable, above-ground pad-mounted equipment and street light poles shall not be placed within sidewalks. They shall not block intersection visibility, and where possible shall be properly screened with landscaping.

3. All development shall conform to or exceed California Green Building Code, Lake Elsinore Municipal Code and any other local requirements for energy conservation.
4. The project proponent/developer(s) shall mitigate project-related public facility impacts through compliance with all applicable City of Lake Elsinore requirements including, but not limited to:
 - a) Payment of all applicable development impact fees and mitigation fees as required by the City of Lake Elsinore Municipal Code and other City-adopted fees.
 - b) Annexation into Community Facilities District No. 2015-1 (Safety) or such other Community Facilities District for Law Enforcement, Fire and Paramedic Services established at the time of such approval to offset the annual negative fiscal impacts of the project on public safety operations and maintenance issues in the City. Alternatively, the project developer may propose alternative financing mechanisms to fund the annual negative fiscal impacts of the project with respect to Public Safety services.
 - c) Annexation into Community Facilities District No. 2015-2 (Maintenance Services) or such other Community Facilities District for Maintenance Services established at the time of such approval to fund the on-going operation and maintenance of the public right-of-way landscaped areas and parks to be maintained by the City and for street lights in the public right-of-way. Alternatively, the project developer may propose alternative financing mechanisms to fund the annual negative fiscal impacts of the project with respect to Maintenance Services.
5. Commercial areas and multiple-family complexes shall contain covered enclosures for collection of recyclable materials.

7 SUSTAINABILITY PLAN

7.1 INTRODUCTION

This chapter provides a framework for development that supports sustainable design practices within the Downtown Elsinore Specific Plan. Included are strategies for energy conservation, programs to promote alternative modes of transportation, reducing solid waste through recycling and reuse, promoting water conservation including water efficient landscape and irrigation design, preserving open space and biological resources, and increasing public health through pedestrian and bicycle connectivity to parks and open spaces.

7.1.1 Relationship to City of Lake Elsinore Climate Action Plan

The City of Lake Elsinore adopted a Climate Action Plan (CAP) on December 13, 2011. The CAP is a strategy for Lake Elsinore to grow in a sustainable way that meets Greenhouse Gas (GHG) reduction goals while continuing to allow for public and private development and redevelopment that will keep Lake Elsinore a vibrant and livable community. The Downtown Elsinore Specific Plan has incorporated a variety of measures throughout the plan to assist in reaching these goals.

7.1.2 Sustainability Guidelines

The Project includes opportunities to increase sustainability and minimize greenhouse gas emissions and reduce water and energy consumption, as well as decrease the impacts of construction activities and waste generation. Presented below is a list of sustainability measures that have been incorporated into the project including those that support the CAP goals.

7.1.2.1 Transportation & Land Use

- ❖ Class II and/or III bicycle paths are within the Downtown Elsinore Specific Plan so that destinations can be reached conveniently by alternative methods of transportation rather than by use of motorized vehicles.
- ❖ All streets within the project include sidewalks on both sides to promote pedestrian access and connectivity between uses.

- ❖ Bicycle parking shall be provided when a new building is constructed, an existing building is enlarged or otherwise structurally altered, or the use or occupancy is intensified, expanded or changed. Bicycle parking may be provided in sheltered, secured facilities located on the project site, in conjunction with off-street parking, or in bike racks on the public sidewalk provided full ADA-compliance pedestrian access is maintained.

7.1.2.2 Energy

- ❖ Energy efficient lighting and control systems will be utilized as an integral part of lighting systems in all buildings. (CAP Measure E-1.3 and Development Standard 25.c: “All new construction shall be consistent with CalGreen Tier 1 energy-efficient building standards through either the performance based or prescriptive approach described in the California Green Building Code. Alternately, a solar photovoltaic system and/or solar water heating may be used to assist in meeting all or a portion of the 15% requirement.”)
- ❖ Light colored “cool” roofs will be required for all new buildings. (CAP Measure E-1.2, and Development Standard 25.b: “Non-residential buildings constructed in the Specific Plan shall use roofing materials having solar reflectance, thermal emittance of Solar Reflectance Index (SRI) 3 or better, consistent with CalGreen Tier 1 values.”)
- ❖ Tree species will be chosen based on their large canopy characteristics at maturity, and will be strategically placed on the west and east portions of the site to shade paving areas and building elevations to minimize heat gain. (CAP Measure E-1.1 and Development Standard 24: “Passive solar heating techniques shall be employed whenever possible within the project. Passive systems involve orienting buildings properly, planting trees to take advantage of the sun, adequate roof overhangs and proper wall insulation.”)
- ❖ Canopies, awnings, and architectural shade structures are encouraged as part of the design guidelines. These design elements will be strategically sized to shade paving areas and building elevations and minimize heat gain.
- ❖ Locally sourced, salvaged and recycled materials will be considered for use throughout the landscape and hardscape design.
- ❖ High efficiency lighting, such as LED, will be utilized for traffic, street and other outdoor lighting. (CAP Measure E-3.2)
- ❖ Lighting levels for outdoor illumination will be required to meet the minimum standards required for safety. All exterior lighting will be required to be controlled by

timers, and unless otherwise required, only lighting required for parking lot security and safety will be provided at night.

- ❖ Building construction in the project shall meet applicable standards for energy efficiency such as:

1. Energy efficient heating and cooling systems;
2. Energy efficient appliances, equipment, and HVAC control systems.

(CAP Measure E-1.3 and Development Standard 25.c: “All new construction shall be consistent with CalGreen Tier 1 energy-efficient building standards through either the performance based or prescriptive approach described in the California Green Building Code. Alternately, a solar photovoltaic system and/or solar water heating may be used to assist in meeting all or a portion of the 15% requirement.”)

- ❖ All new multi-family residential and non-residential development shall include solar photovoltaic systems that meet at least 50 percent of the development’s projected energy use.

7.1.2.3 Solid Waste

- ❖ Individual developers of projects will be encouraged to reuse and recycle construction and demolition waste, including soil, vegetation (green waste), concrete, lumber, metal, and cardboard, to the extent feasible. (CAP Measure S-1.2, CAP Measure S-1.4 and Development Standard 25.e: “Development projects shall be required to divert, recycle or salvage at least 65% of non-hazardous construction and demolition debris generated at the site by 2020 (consistent with CalGreen Tier 1, Section A5.408.3.1). All construction and demolition projects shall be accompanied by a waste management plan for the project and a copy of the completed waste management report shall be provided to the Community Development Department upon completion.”)
- ❖ Individual developers of projects will be encouraged to locate interior and exterior storage bins for recyclables and green waste and adequate recycling containers in public areas.

7.1.2.4 Water

- ❖ Landscaping and irrigation design will meet requirements of the State Water Conservation in Landscaping Act (Government Code Section 65591 et. seq.) by complying with Lake Elsinore Municipal Code (LEMC) Chapter 19.08 (Water Efficient

Landscape Requirements), adopted by the City of Lake Elsinore. (CAP Measure E-4.1)

- ❖ Landscaping will consist of native species selected for water-efficient characteristics and will include drought tolerant planting materials common to the region.
- ❖ Turf requiring irrigation will be discouraged and minimized throughout the project.
- ❖ Irrigation systems and devices will be water efficient and will include satellite soil moisture-based irrigation controls and systems.
- ❖ A purple pipe system will be constructed as part of the infrastructure for the project. Reclaimed water when available will be utilized for landscape irrigation of public and private landscaped areas when available.
- ❖ Watering of non-vegetated surfaces and washing down hard or paved surfaces, including but not limited to sidewalks, walkways, driveways, parking areas, tennis courts, patios or alleys, is prohibited except when necessary to alleviate safety or sanitary hazards, and then only by use of a handheld bucket or similar container, a handheld hose equipped with a positive self-closing water shut-off device, a low volume, high pressure cleaning machine equipped to recycle any water used, or a low volume high pressure water broom. (LEMC Section 19.08.130)
- ❖ Low-impact development practices will be implemented to the extent feasible, to maintain the existing hydrologic character of the drainage and manage and treat storm water to protect the environment.
- ❖ Buildings will be designed to be water-efficient and will include water-efficient fixtures and appliances. (CAP Measure E-4.2 and Development Standard 25.d: “Buildings constructed in the Specific Plan will comply with CalGreen Tier 1 indoor water conservation requirements. Indoor water conservation will be addressed and verified at the Design Review stage.”)

7.1.2.5 Biological Resources

- ❖ The landscape palette should include a large number of native and climate adapted species in an effort to optimize biodiversity, to sequester carbon, and create habitat and minimize resource use (water, fertilizers, and pesticides/herbicides).
- ❖ Invasive species of plants, such as those indicated in the Riverside County Guide to California Friendly Landscapes and the California Invasive Plant Council, shall be avoided especially near parks, buffers, greenbelts, water bodies, and open spaces because of their potential to cause harm to environmentally sensitive areas. (LEMC Section 19.08.070)

- ❖ Storm water best management practices (BMPs) including vegetated bioswales, vegetated detention basins and pervious paving will be incorporated to the extent feasible into individual development sites and along streets. (Drainage Standard 9)

7.1.2.6 Public Health

- ❖ Passive or active recreation opportunities have been incorporated into the design of the project. Walking trails and sidewalks will offer opportunities for exercise.
- ❖ Sidewalks have been included on both sides of all streets. Trails and sidewalks may also be included within the open spaces and utility easements. These proposed improvements will make the project walkable and will provide connections to adjacent development.

7.1.3 California Green Building Code

California Green Building Standards Code (CalGreen Code) is Part 11 of the California Building Standards Code (Title 24, California Code of Regulations) and is the first statewide "green" building code in the United States. The purpose of the California Green Building Code is to improve public health, safety and general welfare by enhancing the design and construction of buildings through the use of building concepts having a reduced negative impact or positive environmental impact and encouraging sustainable construction practices in the following categories:

1. Planning and design
2. Energy efficiency
3. Water efficiency and conservation
4. Material conservation and resource efficiency
5. Environmental quality

The code establishes minimum green building standards for most projects. The Code is composed of several parts with the requirements of:

- ❖ Reducing water consumption by 20 percent, with voluntary goal standards for 30%, 35% and 40% reductions. Replacement of water fixtures with low flow fixtures when new additions are added after January 1, 2017. Dual flush toilets are required.
- ❖ Required diversion of 50 percent of construction waste from landfill, increasing voluntarily to 65% and 75% for new homes and 80% for commercial projects.

- ❖ Installation of low pollutant-emitting materials.
- ❖ Installation of separate water meters for nonresidential buildings' indoor and outdoor water use.
- ❖ Moisture-sensing irrigation systems for larger landscape projects.
- ❖ New construction to add future car charging stations.
- ❖ Mandatory inspections of energy systems (e.g., heat furnace, air conditioner and mechanical equipment) for nonresidential buildings over 10,000 square feet to ensure that all are working at their maximum capacity and according to their design efficiencies.
- ❖ Requiring low-pollutant emitting interior finish materials such as paints, carpet, vinyl flooring and particleboard.
- ❖ Reclaim grey water through filter system for grass, plants and trees; though not allowed for fruits or vegetables. Reuse of black water is not allowed.

All development projects within the Downtown Elsinore Specific Plan will comply with the applicable requirements of the Green Building Code, as implemented by Title 15 of the City of Lake Elsinore Municipal Code.

7.1.4 Green Building Certification Systems

The Downtown Elsinore Specific Plan encourages, but does not require, individual developers to consider the merits of certification through an established green building (sustainability) certification system, not only as a means to conserve energy but also to promote stewardship of the environment and green business practices. The following is a brief description of three of these established systems:

7.1.4.1 LEED

Leadership in Energy and Environmental Design (LEED) is an internationally recognized green building certification system developed by the United States Green Building Council (USGBC), providing third-party verification that a building or community was designed and built using strategies aimed at improving performance across all the metrics that matter most: energy savings, water efficiency, CO₂ emissions reduction, improved indoor environmental quality, and stewardship of resources and sensitivity to their impacts. The United States Green Building Council LEED system of environmental

standards is currently the most recognized system of rating projects and construction. More information regarding this program can be found at <https://new.usgbc.org/leed>.

7.1.4.2 Green Globes® Certification

Green Globes® is a science-based building rating system that supports a wide range of new construction and existing building project types. Designed to allow building owners and managers to select which sustainability features best fit their building and occupants, Green Globes recognizes projects that meet at least 35% of the 1,000 available points. Environmental assessment areas that earn points are energy, indoor environment, site, water, resources, emissions and project/environmental management. The Green Globes program is administered in the United States by Green Building Initiative™, a nonprofit organization and American National Standards Institute (ANSI) Standards Development Organization founded in 2004. The organization is the sole U.S. provider of the Green Globes® and federal Guiding Principles Compliance green building certification programs. Green Building Initiative states that benefits of Green Globes Certification help reduce operating costs, qualify for tax incentives, meet government regulations, attract and retain employees and increase property's marketability. More information regarding this program can be found at <http://www.thegeb.org/green-globes-certification>.

7.1.4.3 The Living Building Challenge™

The Living Building Challenge™ is an international sustainable building certification program created in 2006 by the non-profit International Living Future Institute. The Living Building Challenge™ is a building certification program advocacy tool and philosophy that defines the most advanced measure of sustainability in the built environment possible today and acts to rapidly diminish the gap between current limits and end-game positive solutions.

The Challenge is comprised of seven performance categories called Petals: Place (Site), Water, Energy, Health & Happiness, Materials, Equity and Beauty. Petals are subdivided into a total of twenty Imperatives, each of which focuses on a specific sphere of influence. This compilation of Imperatives can be applied to almost every conceivable building project, of any scale and any location—be it a new building or an existing structure. It is considered to be more rigorous than green certification programs such as LEED. More information regarding this program can be found at <http://living-future.org/lbc>.

8 SPECIFIC PLAN ADMINISTRATION

The Downtown Elsinore Specific Plan is a Year 2040 blueprint for development. The Plan will be implemented through subsequent approval steps and phased development, based on a variety of factors including market demand, financing, absorption, etc. In order for implementation of the Plan to move forward in a proactive manner, implementation tools must include flexibility to respond to changing development conditions over time. The implementation program for the Downtown Elsinore Specific Plan includes a financing and maintenance plan, subsequent development approval, procedures for Specific Plan minor modifications and revisions, and California Environmental Quality Act (CEQA) compliance. These components are described below.

8.1 GENERAL PROVISIONS

8.1.1 Administration

The City of Lake Elsinore shall administer the provisions of the Downtown Elsinore Specific Plan in accordance with the State of California Government Code, Subdivision Map Act, Lake Elsinore General Plan, Lake Elsinore Municipal Code, and other applicable federal, State, and City regulations. Specific Plan development procedures, regulations, standards, and specifications shall supersede the relevant provisions of the City's Municipal Code, as they currently exist or may be amended in the future. Any development regulation or building requirement not addressed in the Specific Plan shall be subject to the City's adopted codes and regulations.

8.1.2 Specific Plan Interpretations

Whenever any ambiguity or uncertainty exists related to this Specific Plan or the application of any of its provisions, the Community Development Director shall make a determination as to the purpose and intent of any disputed provision. Said determination shall be deemed final unless appealed.

8.1.3 Severability

If any provision of this Specific Plan is held to be unconstitutional or otherwise invalid by any court of competent jurisdiction, the invalidity shall not affect any other Specific Plan section, subsection, sentence, clause, or application that can be implemented without

the invalid provision, and to this end, the provisions of this Specific Plan are declared to be severable.

8.2 AFFORDABLE HOUSING

8.2.1 State-mandated Bonus and Incentive Program

The Lake Elsinore Municipal Code (LEMC) includes a bonus and incentive program that satisfies the City's obligations under Government Code Section 65915 et seq. (the "State Density Bonus Law"), as amended from time to time; for the production of housing affordable to a range of incomes, as well as for the creation of senior housing and for the provision of day care facilities. This existing bonus and incentive program allows a developer to receive additional development rights (e.g., density bonus, reduction in site development standards, modification of zoning code or architectural design requirements, reduction in the ratio of parking spaces required) in exchange for the provision of affordable housing. Applicants who seek such density bonuses for housing development within the Downtown Elsinore Specific Plan shall comply with the provisions of the LEMC. Interpretation of the requirements, definitions, and standards for providing density bonuses and incentives for development within the Downtown Elsinore Specific Plan shall be governed by the LEMC as it may be amended for time to time and shall be construed so as not to be inconsistent with the State Density Bonus Law.

8.3 LOT CONSOLIDATION INCENTIVES

8.3.1 Purpose

The presence of small, underutilized lots has been identified as one of the constraints affecting future development in portions of the Downtown Elsinore Specific Plan. It is desirable to encourage the consolidation of smaller lots into larger development sites in order to achieve the scale and quality of development envisioned for the area.

8.3.2 Requirements

In order to qualify for the lot consolidation incentive, a project must include the consolidation of existing small lots into a development site of one acre or greater.

8.3.3 Incentive Bonus

One or more of the following incentives may be available for projects that meet the above consolidation requirement:

1. Increase of maximum floor area ratio (FAR) by 0.50 FAR.
2. Addition of one story to maximum height limit.
3. Density bonus of an additional four dwelling units per acre.
4. Priority in permit processing.
4. If approved by the City Council, a financial incentive, such as the reduction of application fees.

8.3.4 Incentive Process

The project applicant shall submit a written request for Lot Consolidation Incentives to the Community Development Director along with an exhibit showing proposed lot consolidation. The Community Development Director shall determine which incentive bonuses should be granted to the applicant. The extent that such bonuses may be approved may vary on a case-by-case basis.

8.4 PROJECT DEVELOPMENT APPLICATIONS

8.4.1 Project Development Processing Standards

The following processing standards apply to the implementation of future development applications (including tract maps, parcel maps, conditional use permits, design review or substantial conformance) for projects within the Specific Plan area:

1. No development shall occur or building permits shall be issued within the adopted Specific Plan area until an application for the proposed development is submitted for review and is approved by the appropriate entity (as designated by Table 8-1, below) and is found to be consistent with the adopted Specific Plan. Criteria for review and approval of proposed development shall include, but not be limited to, conformance with:
 - a. The Project's land use designation,
 - b. Specific Plan goals, policies, and development standards, and

2. Development within the Specific Plan area shall require individual project review and analysis including General Plan and Specific Plan consistency and conformance with CEQA requirements. Some additional environmental analysis may be required as determined by the Community Development Director.
3. Specific lotting designs and residential dwelling unit types for each planning area shall be determined at the time of individual implementing site design or subdivision proposals. Residential lot sizes, densities, and housing types may vary within each planning area, provided that the cumulative number of units allowed within the Specific Plan is not exceeded.
4. Developers are encouraged to implement the design guidelines provided in Chapter 3 of this Specific Plan.
5. Table 8-1, Implementation Actions/Approval Authority, clarifies the approving authority through the City of Lake Elsinore for various applications and actions.

8.4.2 Tentative Subdivision Maps

A master subdivision map or parcel map (“financing map”), whose purpose is to subdivide large parcels, may be prepared and submitted for review and approval by the Planning Commission and City Council prior to individual tentative map approval. Tentative and Final Subdivision Maps are to be filed with the City for processing in accordance with the provisions of the Subdivision Map Act and the City’s subdivision ordinance. Tentative tract or parcel maps shall be consistent with the Specific Plan.

8.4.3 Design Review

The Design Review process is required to ensure that a proposed development complies with all applicable development regulations, design guidelines, and implementation requirements of the Specific Plan. A Design Review application shall be processed in accordance with the requirements and procedures set forth in the LEMC.

8.4.4 Conditional Use Permits

Applications for Conditional Use Permits (CUPs) will be processed in accordance with the procedures set forth in the LEMC. CUPs will be approved by the Planning Commission. Decisions of the Planning Commission may be appealed to the City Council.

8.5 SPECIFIC PLAN AMENDMENTS AND MODIFICATIONS

8.5.1 Amendments to the Specific Plan

Changes to the Specific Plan that do not qualify as minor modifications under Section 8.4.2 of this Specific Plan require filing of a Specific Plan Amendment (SPA). The following are guidelines for the SPA process:

1. Specific Plan Amendments shall not require a concurrent General Plan Amendment unless it is determined by the City that the proposed amendment would substantively affect General Plan goals, policies, or programs for the Downtown Elsinore Specific Plan area.
2. All Specific Plan Amendments shall be subject to the requirements of the CEQA and any applicable City of Lake Elsinore environmental guidelines.
3. Any amendment to the Downtown Elsinore Specific Plan, even though it may affect only one portion of the Specific Plan, shall be accompanied by a complete specific plan document that includes the entire specific plan, including both changed and unchanged parts.

Specific Plan Amendments shall be acted upon in the same manner that the Specific Plan was adopted, and can be amended as often as deemed necessary by the City Council. The amendment process is generally described as follows:

1. The Planning Commission shall review all proposed amendments to the adopted Specific Plan. Upon the close of the required public hearing, the Planning Commission shall act by resolution to recommend to the City Council approval or denial of the proposed Specific Plan Amendment, or may recommend approval

subject to modifications and conditions.

2. The City Council shall review the Planning Commission's findings and recommendations. Upon the close of the required public hearing, the City Council may approve, approve with modifications or conditions, or deny the proposed Specific Plan Amendment. If approved by the City Council, the proposed Specific Plan Amendment shall be adopted by ordinance.
3. Prior to approving or conditionally approving any Specific Plan Amendment, findings must be made by the Planning Commission and City Council. It must be determined that the Amendment:
 - a. Is consistent with the goals and policies of the Lake Elsinore General Plan and the Downtown Elsinore Specific Plan.
 - b. Results in development of a desirable character that will be compatible with existing and proposed development within the Downtown Elsinore Specific Plan.
 - c. Does not conflict with the goal of creating an environmentally-sensitive, sustainable community.
 - d. Complies with the requirements of the California Environmental Quality Act (CEQA).

8.5.2 Minor Modifications to the Specific Plan

The purpose of Minor Modifications is to provide a ministerial process to determine whether adjustments in Plan provisions and uses or conditions, or situations arise that are not clearly addressed by the Specific Plan, but comply with the intent of the standards, development targets, design concepts, and policy direction of the Specific Plan.

The following minor modifications to the Specific Plan do not require an amendment to the Plan and are subject to review and approval by the Community Development Director ("CDD"). The CDD shall have the discretion to refer a minor modification request to the Planning Commission. A request for a minor modification shall not require a public hearing. The use of Minor Modifications includes, but is not limited to, the following purposes:

1. Determinations regarding issues, conditions, or situations which arise in the implementation of the Plan, and which are not addressed by the Specific Plan.

2. Determinations as to whether a use not listed in Table 3-1, Land Use Matrix, substantially complies with the applicable land use category and is therefore permitted within the Plan as either a permitted use, a use requiring approval of a Conditional Use Permit, or an accessory use or structure.
3. Additions, deletions and changes to the Specific Plan figures or text that substantially comply with the intent of the Specific Plan.
4. Adjustments to the plans or tables contained in Downtown Elsinore Specific Plan, which do not change the conceptual design for the Specific Plan and do not change the requirements for providing adequate infrastructure facilities.
5. Modification of development standards contained in Chapter 3, which do not change the conceptual design for the Specific Plan. The modification shall not exceed a 20 percent change of the affected development standard(s).
6. Slight modifications to Specific Plan boundaries that respond to more accurate or recent data or actual on-site conditions, but which do not increase environmental impacts already assessed in the Project EIR.
7. Minor modifications to the architectural, landscape, or sustainability design guidelines necessary to respond to actual site conditions or to creative new design concepts.
8. Minor modifications to the sewer, water, grading and/or drainage plans that improve efficiency, are necessary to respond to final engineering design, or are required by the City Engineer.
9. Modifications of a similar nature to those listed above, which are deemed minor by the CDD, and which are in keeping with the intent of this Specific Plan.

8.5.2.1 Minor Modification Submittal Requirements

An application for determination of Minor Modification may be submitted in conjunction with an application for Design Review, or at any other time as may be necessary for development. Applications shall be in writing or forms provided by the CDD and shall include at a minimum the following:

1. Name and address of the applicant.
2. Evidence that the applicant is the owner of the property involved or has authorization from the owner to make such application.
3. Location of subject property, including Assessor's Parcel Number(s).
4. Description of proposed facility, use, modification of use, or modification of Specific Plan figure, text, standard(s), or provision(s).
5. If deemed necessary by the CDD, a site plan and/or elevation illustrating the proposed use, development, structure(s), or modification(s) of the Specific Plan. Plans shall be submitted in a format acceptable to the CDD.
6. A written justification of the proposed modification identifying the pertinent portions of the Specific Plan that the proposal is in substantial conformance with. Specific reference should be made to the purpose for the Minor Modification, as described in Section 8.5.2.
7. Such other information deemed necessary by the CDD to determine substantial conformance with the provisions of the Specific Plan.
8. Fees as determined by City Council Resolution for the processing of the application, including referral to the Planning Commission.

8.5.2.2 Minor Modification Findings and Procedures

The CDD shall make the determination of Minor Modification in accordance with the following procedures after making certain findings as described below. The CDD, or his designee, shall approve, approve with conditions, or deny the request based on findings that the request:

1. Substantially conforms with the objectives and policy direction of the Specific Plan, and/or with the intent of applicable provisions if modifications to those provisions are being requested.
2. Will not adversely affect public health and safety.
3. Will not adversely affect adjacent development areas.

4. Complies with the requirements of CEQA.

Such determination shall be made in accordance with the following procedures:

1. If the Minor Modification request is submitted concurrently with an application for Design Review, Conditional Use Permit or subdivision, the determination shall be made in conjunction with the Design Review, Conditional Use Permit or subdivision process.
2. Where no Design Review, Conditional Use Permit or subdivision is involved, determination shall be made according to the following ministerial process:
 - a. Following receipt of an application, the CDD shall inform the applicant in writing within 30 calendar days that the application is complete or that additional information is needed to complete review.
 - b. Within ten business days of determining the application is complete, the CDD shall issue the Substantial Conformance determination.
 - c. Written notice of such determination shall be given to the applicant by mail within seven calendar days after the date of the determination.
 - d. The decision of the CDD shall be final on the expiration of 15 calendar days from and including the date of mailing of the notice of the decision unless a notice of appeal is filed by the applicant with the CDD within such time.

The determination of Minor Modification by the CDD shall be subject to appeal by the applicant to the Planning Commission with the following procedures:

1. The applicant for the determination of Minor Modification may appeal the CDD's decision on the application by filing a written notice of appeal within 15 calendar days of the decision with the CDD, including an appeal fee, prior to the time the decision becomes final.
2. The Planning Commission shall consider the appeal within 30 calendar days of the filing of the appeal with the CDD, or as soon thereafter as possible.
3. The Planning Commission may affirm or modify the determination of Minor Modification by the CDD, and its decision shall be final unless appealed to the City Council.

The determination of Minor Modification by the Planning Commission shall be subject to appeal by the applicant to the City Council with the following procedures:

1. The applicant for the determination of Minor Modification may appeal the Planning Commission's decision on the application by filling a written notice of appeal within 15 calendar days of the decision with the City Clerk, including an appeal fee, prior to the time the decision becomes final.
2. The City Council shall consider the appeal within 30 calendar days of the filing of the appeal with the City Clerk, or as soon thereafter as possible.
3. The City Council may affirm or modify the determination of Minor Modification by the Planning Commission, and its decision shall be final.

8.5.3 Specific Plan Interpretation

In cases of uncertainty or ambiguity as to the meaning or intent of any provision of this Specific Plan, the CDD shall prepare a written interpretation that shall be generally applicable to all future situations of the same type. The interpretation shall be based upon and consistent with the intent of this Specific Plan. The written interpretation shall be transmitted to the affected property owners and any applicant of a pending implementing development proposal within the Specific Plan. Any property owner or applicant may appeal the interpretation to the Planning Commission and to the City Council pursuant to the Minor Modification Procedures listed above.

Table 8-1 Implementation Actions / Approval Authority

Proposal	ACTION REQUIRED BY		
	Community Development Director	Planning Commission	City Council
Design Review	Determination	Appeal	Appeal
Conditional Use Permit		Determination	Appeal
Specific Plan Change – Amendment		Recommendation	Determination
Specific Plan Change – Minor Modification	Determination	Appeal	Appeal
Specific Plan Interpretations	Determination	Appeal	Appeal
Tentative Map(s)		Recommendation	Determination

8.6 MAINTENANCE

Successful operation of maintenance districts and associations is important in ensuring appropriate long-term maintenance of Project sites. Maintenance of private/quasi-public open space and recreation facilities, private roadways, commercial circulation and common landscape areas, and residential common areas will be the responsibility of the residential and commercial associations that are formed within the Specific Plan area. Maintenance responsibilities for public parks, rights-of-way, open space, landscape areas, and street lighting, may be divided among Community Services Districts, Mello-Roos Community Facilities Districts, Landscape and Lighting Districts or other similar associations or districts. Anticipated maintenance responsibilities are further outlined in Table 8-2, Financing and Maintenance Plan.

Maintenance and upkeep of private common areas and amenities must also comply with the Master Association Codes, Covenants, and Restrictions (CC&Rs) that govern the Project Site. The associations shall be responsible for private roads, parking, open space areas, signage, landscaping, irrigation, common areas, on-site sewers, storm drains, retention basins and other responsibilities as necessary.

8.7 FINANCING

8.7.1 Potential Funding Sources

Implementation and long-term maintenance of improvements within the Downtown Elsinore Specific Plan are important considerations to ensure a successful development. Several strategies and tools are available for the financing of new development. It is anticipated that development of the Downtown Elsinore Specific Plan area will require use of mechanisms including, but not limited to, those depicted in Table 8-2, Financing and Maintenance Plan, and further described below.

Table 8-2 Financing and Maintenance Plan

Service or Facility	Construction Responsibility	Funding Responsibility/ Alternatives	Operation & Maintenance Responsibility
Circulation & Transportation			
On-site Local Private Streets and Private Drives	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD*	Property Owners Association
Off-site Streets (if applicable)	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD*	City of Lake Elsinore
Parks, Landscaping			
Internal parkway lighting & landscaping	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD/LLD	Property Owners Association or Landscape & Lighting District
Neighborhood Parks & Paseos	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD/LLD, DV	Property Owners Association or Landscape & Lighting District
Project Entries: Landscaping & Monumentation	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD/LLD	Property Owners Association or Landscape & Lighting District
Utilities			
On-site Public Water Facilities	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD	Elsinore Valley Municipal Water District
On-site Street Lighting	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD	Property Owners Association or Landscape & Lighting District
On-site Wastewater Collection System	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD	Elsinore Valley Municipal Water District
On-site Storm Drainage Facilities	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD	Property Owners Association or City of Lake Elsinore

Service or Facility	Construction Responsibility	Funding Responsibility/ Alternatives	Operation & Maintenance Responsibility
Post Construction Water Quality Facilities	Developer/Builder	Developer/Builder Funding: CL, EQ, CFD	Property Owners Association or City of Lake Elsinore
Electric, Gas, Phone, Cable TV	Developer/Builder	Developer/Builder, Utility Operator/ Funding: CL, EQ, CFD	Utility Operator
Note: Funding Sources noted by abbreviations: CL Conventional Bank Construction Lending EQ Equity Financing CFD/LLD Community Facilities District or Landscape & Lighting District DV Developer Fees			

8.7.1.1 Mello-Roos Community Facilities Act of 1982

The Mello-Roos Act enables cities, counties, special districts, and school districts to establish community facilities districts and to levy special taxes to fund a wide variety of facilities and services required by a specific plan. A Mello-Roos tax can be applied to the planning and design work directly related to the improvements being financed. They may also fund services on a pay-as-you-go basis, including police and fire protection, ambulances, flood protection, recreational programs, parks, and schools. A Mello-Roos district must be established pursuant to the requirements of Government Code Section 53321. As with all special taxes, Mello-Roos taxes are subject to reduction or repeal by initiative.

8.7.1.2 Special Assessment Districts

Special assessment districts, such as those permitted by the Municipal Improvement Act of 1913, the Improvement Bond Act of 1915, and the Lighting and Landscape Maintenance Act of 1972, provide methods of leveraged financing whereby a public entity determines an area in which the provision of facilities will benefit real property. One or more special assessment districts may be created for the Downtown Elsinore Specific Plan, drainage improvements, or other infrastructure. The financing tool can be used for public improvements that directly benefit specific properties that are assessed to pay for the improvements at no risk to public agency general funds.

8.7.1.3 Business Improvement District

A business improvement district (BID) is a geographically defined area within a City, in which services, activities and programs are paid for through a special assessment that is charged to all members within the district in order to equitably distribute the benefits received and the costs incurred to provide the agreed-upon services, activities and programs. The assessment money is collected by the City or by the County through a special contractual arrangement with the city. Because the assessment funds collected in a given district cannot legally be spent outside of that BID, the City creates a trust fund for each BID, with funds periodically released to support operations.

8.7.1.4 Enhanced Infrastructure Financing District (EIFD)

The City of Lake Elsinore is studying the financial feasibility of creating an Enhanced Infrastructure Financing District ("EIFD") around Lake Elsinore to fund infrastructure projects and other public improvements in and around the Lake. The goals of the EIFD and the capital projects funded through the EIFD are:

- ❖ To improve Lake sustainability, including water quality, water levels, flood protection, and lake ecosystem;
- ❖ To enhance public access to and utilization of the Lake (e.g., access, circulation, water storage, recreation); and
- ❖ To support new public and private investment in land development around the Lake to create greater financial capacity for infrastructure investments in and around the Lake.

EIFDs provide a platform and vehicle for local governments to partner with one another to leverage available property tax increment revenues to finance critical infrastructure projects that are of mutual benefit to the participating agencies and their constituents. EIFDs may finance public capital facilities or other projects of community-wide significance that provide significant benefits to the EIFD territory or surrounding community. Projects may include, but are not limited to: highways, interchanges, ramps and bridges, arterial streets, parking facilities, and transit facilities; sewage treatment and water reclamation plants and interceptor pipes; facilities for the collection and treatment of water for urban uses; flood control levees and dams, retention basins, and drainage channels; child care facilities; libraries; parks, recreational facilities, and open space; facilities for the transfer and disposal of solid waste, including transfer stations and

vehicles; brownfield restoration and other environmental mitigation; the development of projects on a former military base; low and moderate income housing subject to affordability covenants and restrictions; acquisition, construction, or repair of industrial structures for private use; transit priority projects located within a transit priority project area; and projects that implement a sustainable communities strategy.

8.7.1.5 Impact Fees and Exactions

Impact fees and exactions are tools used to pay for new development when development results in an increased demand for services. These fees and exactions address the impacts that new development has upon of traffic infrastructure, and general government facilities such as city hall, public works facilities, community center, lakeside facilities, public works facilities, animal shelters and fire facilities. Other fees and exactions collected by the City include park capital improvement fund fees, library capital improvement fund fees, Multiple Species Habitat Conservation Plan (MSHCP) fees, transportation uniform mitigation fees (TUMF), Stephens' Kangaroo Rat Habitat Conservation Plan fees, and drainage improvement fees.

The fees are collected at either prior to the issuance of building permits or prior to final inspection/the issuance of a certificate of occupancy. Because of the timing of collection, impact fee revenues are not available to assist with the construction of infrastructure early in the development process. Developers can receive credit against their impact fee assessments by funding and constructing public infrastructure as part of their overall development plan.

8.7.1.6 Developer Funding

In certain instances, funding for on-site facilities may be tied directly to the Downtown Elsinore Specific Plan project. The developer may pay a fair-share portion of the facility in exchange for development rights. On-site local streets, utility connections from main trunk lines, and drainage facilities are typical examples of facilities that may be funded by the developer. Such improvements will typically be required concurrently with the development.

8.7.2 Infrastructure Financing and Maintenance

1. Local storm drain system shall be funded and constructed by the developer or other mechanism acceptable to the City of Lake Elsinore.

2. Regional storm drain system and flood control improvements associated with the Project shall be funded and constructed by a Community Facilities District or other financing mechanism acceptable to the City of Lake Elsinore.
3. Backbone water facilities and infrastructure shall be owned, operated, and serviced by the Elsinore Valley Municipal Water District. The fair-share cost of designing and constructing the water system shall be borne by the developer.
4. Backbone sewer facilities and infrastructure shall be owned, operated, and serviced by the Elsinore Valley Municipal Water District. The fair-share cost of designing and constructing sewerage improvements shall be borne by the developer.
5. Telephone, electricity, natural gas, and cable television infrastructure shall be installed and maintained by the appropriate utility companies.
6. On-site roadway improvements shall be phased concurrently with adjacent development.
7. A Homeowners Association, Property Owners Association, or other private association or Landscape and Lighting District shall be responsible for installation, maintenance, and upkeep of all common landscape areas, hardscape areas, and irrigation systems within the Specific Plan area.
8. All walkways and multi-purpose paths shall be the responsibility of the project master developer to design, fund, construct, and maintain.
9. All necessary infrastructure (e.g., roads, sewerage, water lines, storm drains, drainage improvements, etc.) shall be phased and installed prior to or concurrently with development.

8.8 CEQA COMPLIANCE

8.8.1 Adoption of Downtown Elsinore Specific Plan

A Program Environmental Impact Report (SCH No. 2005121019) was prepared by the City of Lake Elsinore in accordance with the provisions of the California Environmental

Quality Act (CEQA: California Public Resources Code, Sections 21000 et seq.) and the State CEQA Guidelines (California Code of Regulations, Section 15000 et seq.) to evaluate the potential environmental effects of the City's 2011 General Plan Update, 2008-2014 Housing Element, Climate Action Plan, and a Downtown Master Plan (consisting of the Downtown Master Plan, Downtown Code and Key to Downtown Implementation Plan). The Program Environmental Impact Report (PEIR) was certified by the City Council on December 13, 2018.

The Downtown Elsinore Specific Plan represents a reimagining of the Downtown Master Plan, collates the existing four separate documents into one simplified Specific Plan document, and provides a simplified, more user-friendly document. The Downtown Elsinore Specific Plan also removes property between Spring Street and Ellis Street, and between Flint Street and the Interstate 15 Freeway, from the Downtown Plan in order to recognize existing industrial and commercial land uses that are incompatible with the Downtown Plan. However, the overall development potential within the boundaries of the original Downtown Master Plan, as described in the Specific Plan and by the related General Plan for the property that removed from the Downtown Plan, remains substantially unchanged.

The PEIR presents an analysis of the environmental effects consistent with adoption and implementation of the Downtown Elsinore Specific Plan; specifically evaluating the physical and land use changes from potential development that could occur. Additionally, the PEIR sets out any required mitigation measures needed to minimize or eliminate identified project impacts. The PEIR preparation effort included the preparation and adoption of a Mitigation Monitoring and Reporting Plan (MMRP) in accordance with Section 21081.6 of CEQA and Section 15097 of the State CEQA Guidelines to ensure that the mitigation measures identified in an Environmental Impact Report are implemented.

Accordingly, adoption of the DESP will not result in new or substantially more severe significant effects and no new mitigation measures are necessary. The PEIR, therefore, can serve as the required environmental documentation for the DESP.

8.8.2 Subsequent Individual Projects

The PEIR serves as the required environmental documentation for this Specific Plan, the concurrent General Plan Amendment, and some individual development proposals within the Project area. The PEIR addresses the development of Downtown Elsinore Specific Plan at full buildout, taking into account that development will occur

incrementally over a number of years.

The City intends to use the streamlining and tiering provisions of CEQA to the extent reasonably feasible so that future environmental review of subsequent development projects and public improvement projects can be carried out expeditiously and without the need for repetitive and redundant environmental review.

Thus, for some implementing development projects, the PEIR will assist the City in making environmentally-informed decisions on those projects. When considering these implementing development proposals, the City may determine that the proposed development will not result in new impacts or require additional mitigation triggering the need for a subsequent PEIR as discussed in Section 15162 of the State CEQA Guidelines.

In many cases, site-specific environmental issues will not be known until subsequent design occurs, leading to the preparation of later, project-level environmental documentation. When considering the applicability of the streamlining provisions under CEQA, the City will consider whether such subsequent projects may have impacts which are peculiar to that project or its site, whether the project may result in impacts which were not fully analyzed in the PEIR, or which may result in impacts which are more severe than have been identified in the PEIR. Should any of these factors apply, more detailed project-level environmental review may be required.

As mentioned above, as detailed in Section 15162 of the CEQA Guidelines, subsequent EIRs may be required for projects within this Specific Plan if:

1. Substantial changes are proposed by the implementing project which will require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
2. Substantial changes occur with respect to the circumstances under which the implementing project is undertaken which will require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
3. New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete, shows any of the following:
 - a. The implementing project will have one or more significant effects not

discussed in the previous EIR;

- b. Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- c. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the implementing project, but the implementing project proponents decline to adopt the mitigation measure or alternative; or
- d. Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the implementing project proponents decline to adopt the mitigation measure or alternative.

In cases where some minor technical changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred, the City may require the preparation of an addendum to the previously certified EIR. An addendum need not be circulated for public review but can be included in or attached to the final EIR as described in Section 15164 of the CEQA Guidelines. The decision making body must consider the addendum and the final EIR prior to making a decision on the implementing project as updated by the addendum.

In other cases with more substantive minor technical changes or additions are necessary to make the previous EIR adequately apply to the implementing project in the changed situation (i.e. after years have passed or conditions/project have experienced minor change) and where provisions of Section 15162 are not triggered, the City may require a supplement to the EIR be prepared. A supplement to an EIR shall be given the same kind of notice and public review as is given to a draft EIR. The supplement to an EIR may be circulated by itself without recirculating the previous draft or final EIR. The decision making body must consider the supplement and the final EIR prior to making a decision on the implementing project as updated by the supplement.

9 DEFINITIONS AND ACRONYMS

9.1 DEFINITIONS

“Amusement use” means a facility that provides entertainment activities and diversions such as a Ferris wheel, merry-go-round, or similar structure.

“Antique shop” means a for profit business that engages in the sale of collectibles and other items of value.

“Apartment unit” means one or more rooms with private bath and kitchen facilities comprising an independent, self-contained dwelling unit not owned in fee simple.

“Assembly use” means a meeting, recreational, or social facility of a private or nonprofit organization primarily for use by a member or guests, including union halls, social clubs, fraternal organizations, and youth centers, and “places of religious assembly or institution” as defined in LEMC 17.08.160 (but not including schools as defined in LEMC 17.08.190).

“Automotive repair or service, including tire stores” means a facility providing repair services to automobiles, trucks, or motorcycles, including the sale and mounting of tires.

“Bar” means an establishment primarily serving alcoholic beverages by the drink to the general public, with the service of food incidental to the consumption of liquor. Includes pub, tavern, or cocktail lounge.

“Bed and breakfast” means a home where one family or manager is in permanent residence and from two to six guest rooms are rented for overnight lodging. Meals may be provided for compensation subject to applicable County Health Department regulations, and no provision for cooking in any individual room is made. More than six guest rooms shall be considered a hotel.

“Boardinghouse” means a building or portion thereof where sleeping facilities for three or more persons and meals are provided for compensation on a regular basis.

“CalGreen” means the California Green Building Standards Code (Part 11 of Title 24, California Code of Regulations).

“Car wash” means a permanent, self-service or full-service establishment that provides facilities for car washing.

“Caretaker quarters” means a permanent residence that is provided as an accessory use to a nonresidential use, used as a residence for a caretaker and family for security purposes, or to provide 24-hour monitoring of the site or any person or thing on the site.

“Catering service” means a business that prepares food for consumption off premises.

“Cigar lounge” means an establishment where patrons may purchase and smoke cigars, upon written clearance from the State of California Department of Health.

“Condominium” means a building in which units are owned individually and the structure, common areas and facilities are owned by all the owners on a proportional basis, as generally described in Section 783 of the California Civil Code.

“Day care facility” means a facility that provides nonmedical care and supervision of minor children or elderly adults for periods of less than 24 hours; that does not qualify as a small or large family day care home; and that meets the licensing requirements of the State. Any establishment may provide child or adult day care as an accessory use that is not subject to additional permit requirements; provided, that the establishment offers such service only to its customers or employees, and only during the period when the customers or employees are visiting or working in the establishment.

“Day care, large family” means the use of a residential dwelling unit and the lot upon which it is located for the daytime care of seven to 12 children or elderly adults including any children or elderly adults who normally reside on the premises.

“Day care, small family” means the use of a residential dwelling unit and the lot upon which it is located for the daytime care of six or fewer children or elderly adults including those who reside at the home.

“Day Spa” means a commercial establishment offering cosmetic and therapeutic services such as manicures, pedicures, facial treatments, and massages, without overnight accommodation.

“Duplex” means a structure on a single residential lot of record containing two units for occupancy by two independent households. Each unit is totally separated from the other

by an unpierced wall extending from ground to roof, or an unpierced ceiling and floor extending from exterior wall to exterior wall, except for a common stairwell. Each unit contains its own cooking, sleeping and sanitary facilities and its own entrance. “Triplex” and “Fourplex” have the same definition, with the exception that they have three and four units, respectively.

“Farmer’s market” means a multi-stall market at which farmer-producers sell agricultural products directly to the general public at a central or fixed location, particularly fresh fruit and vegetables (but also meat products, dairy products, and/or grains), and licensed by the City.

“Financial institution” means a facility offering financial services such as a bank, credit union, savings and loan, or finance company.

“Food establishments” means the serving of food and beverages to patrons for compensation, including delicatessens, sandwich shops, bakeries (retail sales only), candy stores, coffee shops, wine tasting, microbreweries, ice cream shops, cafes, and restaurants. Food establishments that serve beer, wine or liquor must be “bona fide eating places” where the gross sales of food prepared and sold to guests on the premises exceeds the gross sales of alcoholic beverages. (i.e., food sales must be 50% or more of the total gross sales of the establishment.)

“Government building & facility” means any facility owned and operated by city, county, state or federal governments, regardless of the use.

“Gun and ammunition sales” means any premises or portions thereof used for the sale, vending, dealing, exchange or transfer of firearms and ammunition.

“Health, fitness or exercise club” means a facility, owned or operated by a corporation, association, person or persons, for a social, educational or recreational purpose, to which membership is required for participation.

“Home occupation” means a use conducted entirely within a dwelling and carried on by the inhabitant(s) thereof, which use is clearly incidental and secondary to the use of the structure for residential purposes and which does not alter the character thereof. By its very nature and scope, the use does not adversely affect the activities, enjoyment or property values of the neighborhood.

“Hookah bar” means a commercial establishment where people gather to smoke flavored tobacco from a hookah pipe.

“Hotel” means a building or portion thereof, or a group of buildings, with access through a common entrance, lobby or hallway, to seven or more guest rooms that are designed and intended for paid overnight lodging on a short-term basis.

“Housing, permanent” includes apartments, condominiums, duplexes, triplexes, fourplexes, row houses, and townhouses.

“Kiosk for retail sales” means any portable, non-motorized, non-wheeled, semi-permanent structure used for the sale of retail goods, food, and/or beverages, and licensed by the City.

“Library” means a public, quasi-public or privately owned facility that is open to the public for the primary use of literary, musical, artistic or reference materials.

“Live/work unit” means buildings or spaces within buildings that are used jointly for commercial and residential purposes where the residential use of the space is secondary or accessory to the primary use as a place of work.

“Massage establishment” means any fixed place of business where any individual, firm, association, partnership, or corporation engages in, conducts, or permits massages or health treatments involving massage on the premises.

“Medical office” means an establishment that provides medical services by medical professionals such as physicians, dentists, chiropractors, optometrists or other similar medical staff. Includes a clinic, medical or dental laboratory or pharmacy incidental to an office, dialysis center, outpatient surgicenter, or urgent care. In the mixed-use zones, hours of operation limited to 7:00 a.m. until 10:00 p.m.

“Mineral spa/Hot springs spa” means a resort developed around naturally occurring mineral springs or hot water sources, with invigorating baths, or a place with therapeutic services such as massages, saunas, baths and manicures.

“Motel” means a lodging establishment typically featuring a series of rooms whose entrance is immediately adjacent to a parking lot, as might facilitate easy access to one's automobile during an overnight stay.

“Museum” means a building serving as a repository for a collection of natural, scientific or literary curiosities or objects of interest, or works of art, arranged, intended and designed to be used by the public for viewing. The sale of goods to the public as gifts may be included as an accessory use.

“Nightclub” means a commercial establishment dispensing alcoholic beverages for consumption on the premises and in which dancing and musical entertainment are permitted. Includes discotheques and dance halls but does not include any adult-oriented business.

“Park” means a noncommercial public outdoor recreational area that provides active or passive activities. Includes playgrounds.

“Parking facility” means an open, paved or otherwise surfaced area, or a structure, used for the parking and storage of vehicles, for free or for compensation or to accommodate patrons, customers or clientele of a business.

“Pawn shop” means an establishment that lends money at a specified rate of interest on articles of personal property left as security. Does not include gun sales unless a conditional use permit application is filed, reviewed, and approved by the Planning Commission.

“Personal service” means an establishment that offers services to its customers by an act or useful labor, including a barber shop, beauty salon, day spa, manicure, tailor, dry cleaning, laundromat, small appliance repair (household, television, stereo system, computer), watch and jewelry repair, shoe repair, tax preparation, travel agency, employment agency, ticket agency, copy center, printing, automobile rental (office only), computer rental, cyber cafe, or package, postal and mailbox service.

“Pet shop” means an establishment engaged in the retail sale of animals, pet supplies or grooming, including exotic animals, which are defined as not commonly domesticated in the United States or are wild by nature. No boarding of animals is permitted.

“Pollutant” means anything which causes the deterioration of water quality such that it impairs subsequent and/or competing uses of the water. Pollutants may include but are not limited to paints, oil and other automotive fluids, soil, rubbish, trash, garbage, debris, refuse, waste, fecal coliform, fecal streptococcus, enterococcus, heavy metals, hazardous waste, chemicals, fresh concrete, yard waste from commercial landscaping operations,

animal waste, materials that result from the process of construction a building or structure, nauseous or offensive matter or any kind.

“Professional office” means an office used by persons who provide services that require a state license or certificate, such as accountants, architects, planners, engineers, surveyors, geologists, lawyers, designers, real estate agents, investment brokers, title and escrow offices, and business incubators, but does not include a veterinary office or kennel, nor any adult-oriented business.

“Recreation facility” means an establishment that provides entertainment activities or services for a fee or admission charge, including a bowling alley, electronic game arcades, billiard rooms, miniature golf, or indoor party center, but does not include any adult-oriented business.

“Retail sale” means the selling of goods, wares or merchandise directly to the ultimate consumer or persons without a resale license. Establishments may include convenience mart, florist or flower shop, neighborhood grocery, insurance, pharmacy, jewelry, apparel, gift, hobby, hardware, bookstore, stationery, art exhibit and gallery, sporting goods, bicycle and bicycle rental, toy, music store (including sale of instruments, records, tapes), vehicle parts (excluding repair and service), and antiques.

“Row house/townhouse” means a single-family attached dwelling in a row of such units in which each unit has its own front and rear access to the outside and no unit is located over another.

“Rooftop Use” means a use placed upon the roof of a building that is appurtenant to a permitted use within the same building, including but not limited to, roof terraces for residential projects, rooftop bars, lounges, and dining for hotels and restaurants, rooftop gardens, and green roofs.

“Specialty Food Shop” means a small establishment selling unique or quality items such as spices, coffees, olive oils, soaps, nuts, cheeses, health foods, and the like. It does not include a grocery store.

“Street vendor” means a portable, nonpermanent, wheeled, movable facility for the sale of retail merchandise or food and beverages from a wagon or pushcart by a vendor with a valid City business license.

“Studio/workshop” means a studio facility for the arts, crafts, photography, music and music recording, dance, gymnastics, aerobics, yoga, martial arts, cooking.

“Swap meet” means an outdoor area in which stalls or sales areas are set aside, rented or otherwise provided, which are used by unrelated individuals to sell, trade, exchange, place on consignment, or swap articles that are either new, homemade, homegrown, old, antique, or obsolete. Includes flea market or farmers’ market.

“Theater” means a building or portion thereof for the showing of motion pictures, or for dramatic, dance, musical or other live performances.

“Thrift store” means a for profit or nonprofit business that collects donated items and engages in the sale or resale of previously owned or used goods and merchandise, and does not include antique shops.

“Wireless communications facility” means any antenna, associated equipment, base station, small-cell system, tower, and/or transmission equipment.

9.2 ACRONYMS

The following is a list of acronyms used in this document.

❖ AFV	Animals Friends of the Valley
❖ ANSI	American National Standards Institute
❖ BID	Business Improvement District
❖ BMP	Best Management Practice
❖ Cal Fire	California Department of Forestry and Fire Protection
❖ Cal Green	California Green Building Standards Code, Part 11 of the California Code of Regulations
❖ CAP	Climate Action Plan
❖ CDD	Community Development Director
❖ CEQA	California Environmental Quality Act
❖ CC&Rs	Codes, Covenants, and Restrictions
❖ CUP	Conditional Use Permit
❖ EIFD	Enhanced Infrastructure Financing District
❖ EIR	Environmental Impact Report
❖ EVMWD	Elsinore Valley Municipal Water District
❖ FAR	Floor Area Ratio
❖ GHG	Greenhouse Gas
❖ GP	General Plan
❖ HCOC	Hydraulic Conditions of Concern
❖ I-15	Interstate 15
❖ LED	Light-emitting diode
❖ LEED	Leadership in Energy and Environmental Design
❖ LEMC	Lake Elsinore Municipal Code
❖ LEMSAR	Lake Elsinore Marine Search and Rescue

❖ LEUSD	Lake Elsinore Unified School District
❖ LID	Low Impact Development
❖ MDP	Master Drainage Plan
❖ MRF	Materials Recovery Facility
❖ MG	Million gallons
❖ MMRP	Mitigation Monitoring and Reporting Program
❖ MS4	Municipal Separate Storm Sewer System
❖ MSHCP	Western Riverside County Multiple Species Habitat Conservation Plan
❖ NPDES	National Pollutant Discharge Elimination System
❖ PEIR	Program Environmental Impact Report
❖ PHHWCF	Lake Elsinore Regional Permanent Household Hazardous Waste Collection Facility
❖ PRC	Public Resources Code
❖ RCDWR	Riverside County Department of Waste Resources
❖ RCFD	Riverside County Fire Department
❖ RCWD	Rancho California Water District
❖ SP	Specific Plan
❖ SPA	Specific Plan Amendment
❖ SR-74	State Route 74
❖ SRI	Solar Reflectance Index
❖ TUMF	Transportation Uniform Mitigation Fees
❖ USGBC	United States Green Building Council
❖ WQMP	Water Quality Management Plan
❖ WRF	Water Reclamation Facility
❖ WTP	Water Treatment Plant

APPENDIX A
Consistency with the
General Plan

APPENDIX A

Consistency with the General Plan

The Downtown Elsinore Specific Plan provides a vision and strategic framework to guide the future development of the City's historic downtown.

All specific plans are required by California Government Code Section 65454 to be consistent with the adopted General Plan of the jurisdiction within which the Project is located. The Downtown Elsinore Specific Plan will implement the policies of the Lake Elsinore General Plan. It shall provide a link between the policies of the General Plan and individual development proposals within the Specific Plan area. All subsequent subdivision and parcel maps, all public and private development projects, and discretionary permits within the boundaries of the Downtown Elsinore Specific Plan (Plan) must be consistent with the Plan.

The format of the following analysis is:

- Restatement of the applicable General Plan goals and policies.
- Statement regarding whether the Specific Plan is consistent with, inconsistent with, or not applicable to the goal or policy.
- Discussion regarding the Specific Plan's consistency with the goal or policy.

Table A-1, Consistency with the General Plan adopted December 2011

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
Chapter 2	COMMUNITY FORM	
2.3.4	LAND USE GOALS AND POLICIES	
Goal CF 1	Create a diverse and integrated balance of residential, commercial, industrial, recreational, public and open space land uses.	CONSISTENT. The Plan includes mixed-use, medium- and high-density residential, commercial, business professional, downtown recreational, public institutional, and open space land uses for the enjoyment of both residents and tourists visiting the City.
CF 1.1	Promote innovative site design, and encourage the preservation of unique natural features, such as steep slopes, watercourses, canyons, ridgelines, rock formations, and open space with recreational opportunities	CONSISTENT. The Plan promotes integrated, walkable, mixed-use development that preserves the historic downtown, while linking the City's great Main Street to its most valued resource, Lake Elsinore itself.
CF 1.2	Encourage development of unified or clustered community level and neighborhood level commercial centers and discourage development of strip commercial uses.	CONSISTENT. The Plan utilizes the mixed-use land use designation to encourage service commercial businesses integrated with residential development. Furthermore, the Plan identifies the Downtown Core as a focal point for 18-hour commercial uses that promotes a vibrant Main Street.
CF 1.3	Encourage the development of sit-down restaurant establishments where appropriate and discourage the proliferation of drive-through fast food establishments.	CONSISTENT. The Plan provides opportunities for a full range of eating establishments that serve both residents and tourists. However, drive-through services (including fast food) are not allowed in the Downtown Core area, between Heald Avenue and Library Street.
CF 1.4	Encourage development of a mix of industrial uses	NOT APPLICABLE. The Plan is consistent with the land uses proposed in the General

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	including light industrial, clean manufacturing, technology, research and development, medium-industrial, and extractive uses.	Plan for the Historic District, which does not anticipate the development of uses suggested in this policy within the Plan's boundaries.
CF 1.5	Encourage the development of large planned industrial and/or professional office parks on large parcels.	NOT APPLICABLE. The Plan is consistent with the land uses proposed in the General Plan for the Historic District, which does not contain large parcels appropriate for such uses within the Plan's boundaries.
CF 1.6	Encourage development of institutions including hospitals and educational campuses and facilities.	NOT APPLICABLE. The Plan is consistent with the land uses proposed in the General Plan for the Historic District, which does not anticipate the development of uses suggested in this policy within the Plan's boundaries.
CF-1.7	Encourage the use of paseos, green belts, linear parks, and trails within future developments.	CONSISTENT. The Plan strives for a walkable environment with development incorporating pedestrian paths, hiking trails and bicycle lanes. A major portion of the Riverwalk along the Outlet Channel lies within the boundaries of the Plan. Through the design review process, green amenities can be implemented.
CF 1.8	Encourage a jobs/housing balance of one job for every 1.05 households by the year 2030.	CONSISTENT. Development in the Plan will generate jobs that will assist the City in achieving its jobs/housing balance target.
CF 1.9	Encourage rehabilitation and new construction to replace aging commercial facilities.	CONSISTENT. The Plan contains a flexible land use plan to encourage investment in the downtown area, and flexible architectural principles and guidelines for rehabilitation and new construction of buildings. The Plan provides a simplified and user-friendly

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		document that streamlines the development process.
Goal CF 2	Establish and maintain the City as a year-round recreation destination.	CONSISTENT. Development of the Plan will result in a Great Main Street with nighttime entertainment. The Downtown Recreational designation adjacent to the Lake promotes year-round recreational uses and leisure activities for City residents and visitors.
CF 2.1	Encourage recreational uses including parks, beaches, marinas, motocross, soaring, skydiving, and a multipurpose trail within the City's rights of way.	CONSISTENT. The Plan retains the Downtown Recreational designation for the area adjacent to the Lake, specifically for parks, public and private open space, passive and active recreation, combined with limited retail, dining, entertainment, cultural, lodging, water-oriented recreational commercial uses and special events.
CF 2.2	Consider the feasibility and encourage if feasible the development of a new pier near the end of Main Street to connect the lake with the Downtown area.	CONSISTENT. The Plan identifies the importance of the connection from the Downtown to the Lake.
CF 2.3	Consider the feasibility of development of geothermal resources such as a spa or bathhouse establishment in the Downtown area.	CONSISTENT. The Plan identifies the importance of past uses and permits Mineral Spa/Hot Springs Spa uses.
CF 2.4	Continue to evaluate the provision of public access to the lake and open space areas when making land use decisions.	CONSISTENT. The Plan identifies the importance of the connectivity and access to the Lake, and through the design review process, can ensure that development provides them.
CF 2.5	Encourage a pedestrian circulation route around the	CONSISTENT. The Plan supports pedestrian access and circulation within the

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	lake to improve public access to this amenity.	Downtown Recreational designation along the Lake.
Goal CF 3	Establish a development pattern that preserves aesthetics and enhances the environmental resources of the City.	CONSISTENT. The Plan includes design and landscape standards to ensure a high quality of development, with strong visual and physical relationships with the Lake, the City's most important environmental resource.
CF 3.1	Upon availability of appropriate funding, the City shall establish hillside grading standards that address unique natural features and encourage the sensitive treatment of hillsides in the site design and architecture of new construction.	NOT APPLICABLE. The downtown area covered by the Plan is relatively flat, and new hillside grading standards are not required for development to occur.
CF 3.2	Encourage new commercial and/or industrial developments to incorporate buffers which minimize the impacts of noise, light, visibility, or activity and vehicular traffic on residential uses and MSHCP conservation areas.	CONSISTENT. Areas designated for non-residential uses within the Plan adjacent to residential or conservation uses are required to provide a buffer to separate land uses and mitigate impacts.
Goal CF 4	Develop a viable downtown area that preserves potentially significant historical structures and provides civic and cultural opportunities as well as a destination for shopping, meeting, and gathering for both tourists and residents.	CONSISTENT. The Plan identifies historic structures within and in proximity to the downtown area. Protection of existing noteworthy structures and respect for the City's heritage is a theme that will guide new development. The land use plan provides cultural, civic meeting, shopping and gathering opportunities for tourists, shoppers, and residents.

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CF 4.1	Encourage the historic registration of potentially significant historic buildings as identified in Section 4.7 of the General Plan.	CONSISTENT. The Plan identifies existing potentially significant historic buildings.
CF 4.2	Encourage the preservation, innovative reconstruction and reuse of historic buildings in and around the Historic District.	CONSISTENT. The Plan calls for an eclectic blend of both the historic buildings and modern designs that reflect efficiency, innovation, and uniqueness. This flexibility allows opportunities to utilize and preserve features that best accommodate their use, location and market base.
CF 4.3	Consider locating additional civic, public, and cultural facilities and encourage both residential and commercial mixed uses, in and around the Main Street Overlay area.	CONSISTENT. The Plan utilizes the mixed-use land use designation particularly along both sides of Main Street to encourage additional civic, public and cultural facilities, as well as residential and commercial development.
CF 4.4	Encourage the revitalization of the Historic District through the revisions of the Historic Elsinore Architectural Design Guidelines and implementation of the Downtown Master Plan.	CONSISTENT. The Downtown Elsinore Specific Plan implements the vision and objectives formerly created by its predecessor, the Downtown Master Plan. However, the Plan has specified standards and guidelines that simplify and replace the outdated Historic Elsinore Architectural Design Standards originally adopted in 1993.
Goal CF 5	Promote land use strategies that decrease reliance on automobile use, increase the use of alternative modes of transportation, maximize efficiency of urban service provision and reduce emissions of greenhouse gas emissions,	CONSISTENT. The Plan's objective is to provide a walkable downtown with bikeways, public transit services, and public parking facilities strategically located to encourage "park-once-and-walk."

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	as detailed in the City's Climate Action Plan.	
2.4.4	CIRCULATION GOALS AND POLICIES	
Goal CF 6	Optimize the efficiency and safety of the transportation system within the City of Lake Elsinore	CONSISTENT. The Plan retains the existing truck routes that serve industrial uses off Main Street.
CF 6.1	The interconnection and coordination of traffic signals shall be achieved through two processes, namely the requirements in the conditions of approval on development projects and/or through the implementation of Capital Improvement Program projects.	CONSISTENT. Development in the Plan area will comply with all City requirements related to the interconnection and coordination of traffic signals. The coordination of closely spaced signals is standard practice that will occur at the design stage, if needed. A Traffic Management Plan shall be prepared for event traffic if needed and implemented for applicable land uses. The Traffic Management Plan will also address coordination of traffic signals if necessary.
CF 6.2	Enforce and comply with proper intersection "sight distance" requirements as described by the Engineering Division.	CONSISTENT. The transportation network that serves the Plan area is existing. Future development is subject to compliance with sight distance requirements established by the City.
CF 6.3	Maximize the use of shared driveways and on-site circulation to minimize conflicts at access points to the roadway network.	CONSISTENT. The Plan limits driveways along Main Street, and encourages the use of alleys and shared driveways off Main Street.
CF 6.4	Maintain the system of bike lanes and multi-use trails throughout the City. Encourage the implementation of the network of Class I, II, and III bike lanes in all	CONSISTENT. The transportation network that serves the Plan area is existing. The Plan identifies existing and future bikeways, including Class III bike lanes on Main Street and Graham Street, and Class II bike lanes along the Lake on Lakeshore Drive and Limited Avenue, on Pottery Street and on

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	development project through construction of the facility as described in the Bike Lane Master Plan and/or the Trails Master Plan.	Sumner Avenue. The Plan likewise supports the City's Trails Master Plan with the Riverwalk trail system along the Outlet Channel.
CF 6.5	The City will monitor traffic and congestion on Grand Avenue and Corydon Street through the review of project-specific traffic studies, and apply mitigation measures to ensure that projected traffic does not exceed daily capacities as new development occurs in the area.	NOT APPLICABLE. Grand Avenue and Corydon Streets are not within the boundaries of the Plan, nor, due to their location, is it likely that development in the downtown area will affect these roadways.
CF 6.6	As appropriate, coordinate City improvements with the efforts of the County and adjacent cities that provide a circulation network which moves people and goods efficiently to and from the City.	CONSISTENT. Development in the downtown area utilizes the existing roadway system defined in the General Plan, which considers networks of the County and adjacent cities.
2.5.2	GROWTH MANAGEMENT GOALS AND POLICIES	
Goal CF 7	Maintain orderly, efficient patterns of growth that enhance the quality of life for the residents of Lake Elsinore.	CONSISTENT. The Plan focuses on the unique recreational, leisure, shopping, and housing opportunities within its core downtown area that will contribute to the quality of life in Lake Elsinore. The Plan is consistent with the land uses proposed in the General Plan for the Historic District.
CF 7.1	Encourage mixed-use developments to reduce	CONSISTENT. The Plan emphasizes Mixed-Use development within the

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	public service costs and environmental impacts through compatible land use relationships, and efficient circulation and open space systems.	downtown area, particularly on both sides of Main Street.
2.7.3	PARKS AND RECREATION GOALS AND POLICIES	
Goal CF 8	Maintain an adequate quantity, quality, type, and distribution of parks and recreational facilities throughout the City that serve the current and future needs of residents and visitors.	CONSISTENT. The Plan provides opportunities to develop a wide range of recreational facilities including parks, public and private open space, passive and active recreation, and water-oriented recreational uses and special events
CF 8.1	Continue to utilize the City of Lake Elsinore Parks and Recreation Master Plan as a guide for decision-making and implementation of the Parks and Recreation Program.	CONSISTENT. The Plan anticipates the development of a wide-range of recreational facilities within the Downtown Recreational designation, to utilize its location adjacent to the Lake.
CF 8.2	Ensure parkland and recreation facilities support new development through acquisition and/or dedication. Meet the requirement of the Park Capital Improvement Fund, Resolution No. 91-42 and the Quimby Act by acquiring five (5) acres of useable parkland per 1,000 population.	CONSISTENT. The Plan provides opportunities to develop a wide-range of recreational facilities. Parks and/or payment of Park and Quimby fees shall be included in the design of future residential development projects.
CF 8.3	Explore the use of public-private partnerships,	CONSISTENT. The Plan allows for the pursuit of public-private partnerships,

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	corporate sponsorships, and leasing agreements that provide for additional parks and recreational facilities, and other programs including cooperation with applicable school districts to allow joint use of facilities.	corporate sponsorships and leasing agreements with development in the downtown area. Additionally, a Public Art Program will be pursued to enhance the downtown's public realm.
CF 8.4	Consider the addition of a centralized forum for community activities such as a convention center, amphitheater, cultural arts center, or waterfront promenade.	CONSISTENT. The Plan provides opportunities for an entertainment venue, convention center, waterfront promenade, or other public spaces.
CF 8.5	Continue to develop public beach lands and recreational facilities such as campgrounds, boat launch areas, and water access to the Diamond Stadium, to complement the state and county recreation system.	CONSISTENT. The Plan recognizes the importance of the Lake and other sporting venues in the City, including the Diamond Stadium. The Plan's Downtown Recreational designation allows boat dock and pier uses that promote water access around the Lake. The Plan also encourages a Bike Share Program, trolley service and a water taxi station to connect to the Diamond Stadium, the Launch Pointe Recreation Destination & RV Park and other points of interest in the City.
CF 8.6	Encourage the development of private recreational facilities within residential and mixed-use developments.	CONSISTENT. Private recreational facilities will be provided within residential and mixed-use areas, in accordance with the Plan and with Lake Elsinore Municipal Code requirements.
CF 8.7	Ensure that recreation facilities are accessible to the elderly, children, and persons with disabilities as set forth in the Americans	CONSISTENT. All development within the Plan area will be consistent with the requirements of the Americans with Disabilities Act.

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	with Disabilities Act including increased wheelchair access, height variations on drinking fountains, and any other requirements necessary to service these individuals.	
Goal CF 9	Establish a primary trail network for equestrians and hikers.	CONSISTENT. The Plan supports existing and future biking and hiking trails systems within its boundaries.
CF 9.1	Encourage public and private systems that interface with other existing and proposed trails (i.e., bikeways) assuring links with the City, County of Riverside, and state recreational facilities.	CONSISTENT. See response to Goal CF 6.4 and CF-9 above.
Chapter 3	PUBLIC SAFETY AND WELFARE	
3.2.3	AIR QUALITY GOALS AND POLICIES	
Goal PS 1	Continue to coordinate with the Air Quality Management District and the City's Building Department to reduce the amount of fugitive dust that is emitted into the atmosphere from unpaved areas, parking lots, and construction sites.	CONSISTENT. Future development envisioned by the Plan will be required to comply with all regulatory requirements regarding the reduction of fugitive dust. In accordance with established regulations, all property owners shall be required to maintain vacant properties in a manner that reduces the amount of fugitive dust that is emitted from unpaved areas.
PS 1.1	Continue to implement requirements identified in the National Pollutant Discharge Elimination System (NPDES).	CONSISTENT. All development in the Plan area will be fully compliant with the requirements of the NPDES.

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Goal PS 2	Work with regional and state governments to develop effective mitigation measures to improve air quality.	CONSISTENT. The City has worked with regional and state agencies in the development of its Climate Action Plan. Development projects in the Plan shall comply with the CAP strategies and measures.
PS 2.1	Support the SCAQMD in its development of improved ambient air quality monitoring capabilities and establishment of standards, thresholds, and rules to address, and where necessary mitigate, the air quality impacts of new development.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
PS 2.2	Support programs that educate the public about regional air quality issues, opportunities, and solutions.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, whenever feasible development projects in the Plan will be encouraged to support applicable educational programs.
PS 2.3	Evaluate the purchase of alternative fuel vehicles for official City vehicles.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
3.3.3	HAZARDS/HAZARDOUS MATERIALS GOALS AND POLICIES	
Goal PS 3	Reduce the level of risk associated with the use, transport, treatment, and disposal of hazardous materials to protect the community's safety, health, and natural resources.	CONSISTENT. Any development within the Plan is required to comply with Federal, State and local hazardous material requirements that ensure the protection of the community's safety, health, and natural resources.
PS 3.1	Continue to require hazardous waste generators to implement a waste	CONSISTENT. Where necessary, development projects in the Plan area shall comply with the Riverside County

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	reduction program per the Riverside County Hazardous Waste Management Plan with necessary inspections per the Riverside County Hazardous Materials Handlers Program.	Hazardous Waste Management Plan, including waste reduction programs for hazardous waste generators.
PS 3.2	Require any proposed development within close proximity to an active and/or inactive landfill to complete a technical analysis that focuses on public safety and hazard issues. The analysis shall be prepared by a professional consultant.	NOT APPLICABLE. No active and/or inactive landfills are located in close proximity to the downtown area.
PS 3.3	Encourage the safe disposal of hazardous materials with County agencies to protect the City against a hazardous materials incident.	CONSISTENT. All development within the Plan area will comply with applicable regulations regarding disposal of hazardous materials if any are used. See response to Goal PS 3.
PS 3.4	Continue operating household hazardous waste education and collection programs in collaboration with the Riverside County Department of Environmental Health.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, whenever feasible development projects in the Plan area will be encouraged to support applicable educational programs and promote City collection programs.
PS 3.5	Evaluate new development on or adjacent to the Santa Ana Regional Interceptor (SARI) line requiring extensive subsurface components or containing sensitive land uses such as	NOT APPLICABLE. The Plan area is not on or adjacent to the SARI line.

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	schools on a project-by-project basis to determine impacts if an accident occurs.	
3.4.1	WILDFIRE HAZARDS GOALS AND POLICIES	
Goal PS 4	Adhere to an integrated approach to minimizing the threat of wildland fires to protect life and property using pre-fire management, suppression, and post-fire management.	CONSISTENT. The downtown is not generally an area that is subject to wildland fires. However, the General Plan identifies some moderate and high fire hazard zones within the Plan's boundaries. Development projects shall adhere to the latest fire management techniques, including the provision of onsite Fuel Modification Zones, if deemed necessary by the Fire Marshal.
PS 4.1	Require on-going brush clearance and establish low fuel landscaping policies to reduce combustible vegetation along the urban/wildland interface boundary.	CONSISTENT. Properties within the Plan are subject to Citywide brush clearance requirements.
PS 4.2	Create fuel modification zones around development within high hazard areas by thinning or clearing combustible vegetation within 100 feet of buildings and structures. The fuel modification zone size may be altered with the addition of fuel resistant building techniques. The fuel modification zone may be replanted with fire-resistant material for aesthetics and erosion control.	CONSISTENT. Properties within the Plan are subject to the City's fuel modification requirements. Development shall be conditioned to provide fuel resistant building materials and construction, and plant fire resistant landscape species for erosion control.

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PS 4.3	Establish fire resistant building techniques for new development such as non-combustible wall surfacing materials, fire-retardant treated wood, heavy timber construction, glazing, enclosed materials and features, insulation without paper-facing, and automatic fire sprinklers.	CONSISTENT. All development within the downtown area will comply with applicable building code requirements for fire resistance.
PS 4.4	Encourage programs that educate citizens about the threat of human wildfire origination from residential practices such as outdoor barbeques and from highway use such as cigarette littering.	CONSISTENT. Project developers within the Plan, in cooperation with the City, will distribute information regarding the threat of human wildfire origination from residential practices when available.
3.5.10	FLOODING AND FLOODPLAINS GOALS AND POLICIES	
Goal PS 5	Minimize the risk of injury to residents and visitors and of property damage due to flooding.	CONSISTENT. Flood zones are found in the Plan area, but are limited to that area in proximity to the designated Floodway. Land uses proposed in areas subject to flooding will be designed to prevent injury to residents and visitors.
PS 5.1	Continue to ensure that new construction conforms to all applicable provisions of the National Flood Insurance Program in order to protect buildings and property from flooding.	CONSISTENT. Development in the Plan will conform to all applicable provisions of the National Flood Insurance Program and to LEMC Sections 15.64 – Flood Damage Prevention and 15.68 – Floodplain Management. See consistency discussion for Goal PS 6.
PS 5.2	Utilize the Capital Improvement Program for storm drainage projects and	CONSISTENT. The City has prepared a Draft Master Drainage Program that includes the downtown area. Improvement

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	maintenance and improvement of local storm drain systems including channels, pipes, and inlets to ensure capacity for maximum runoff flows.	and maintenance costs of improvements will be included in the CIP and paid for by development project fees and other funding.
3.6.11	SEISMIC ACTIVITY GOALS AND POLICIES	
Goal PS 6	Minimize risk of loss of life, injury, property damage, and economic and social displacement due to seismic and geological hazards resulting from earthquakes and geological constraints.	CONSISTENT. A portion of the Alquist-Priolo Special Studies Zone for the Elsinore Fault runs along Lakeshore Drive. Development in the Plan will comply with City and state building and seismic safety requirements that reduce the risk of loss of life, injury, property damage, and economic and social displacement due to seismic and geological hazards.
PS 6.1	Encourage the pursuit of federal and state programs that assist in the seismic upgrading of buildings to meeting building and safety codes.	CONSISTENT. Land use proposals in the Plan area will be required to meet current building and safety codes including seismic hazard protection. The City will distribute state and federal grant program information when applicable.
PS 6.2	Continue to require Alquist-Priolo and other seismic analyses be conducted for new development to identify the potential for ground shaking, liquefaction, slope failure, seismically induced landslides, expansion and settlement of soils, and other related geologic hazards for areas of new development in accordance with the Fault Rupture Hazard Overlay District adopted by the City	CONSISTENT. See consistency discussion for Goal PS 6.

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	of Lake Elsinore Zoning Code. The City may require site-specific remediation measures during permit review that may be implemented to minimize impacts in these areas.	
3.7.30	NOISE GOALS AND POLICIES	
Goal PS 7	Maintain an environment for all City residents and visitors free of unhealthy, obtrusive, or otherwise excessive noise conditions.	CONSISTENT. Proposed uses could be significant noise producers. Such uses should be located away from noise-sensitive receptors. Residential areas shall be protected by appropriate noise mitigation measures to ensure noise standards are not exceeded, including open space or structural buffers.
PS 7.1	Apply the noise standards set forth in the Lake Elsinore Land Use Noise Compatibility Matrix (General Plan Table 3-1) and Interior and Exterior Noise Standards (General Plan Table 3-2) when considering all new development and redevelopment proposed within the City.	CONSISTENT. See consistency discussion for Goal PS 7. Noise sensitive uses within the Plan area shall be protected by appropriate noise mitigation measures to ensure noise standards are not exceeded.
PS 7.2	Require that mixed-use structures and areas be designed to prevent transfer of noise and vibration from commercial areas to residential areas.	CONSISTENT. See consistency discussion for Goal PS 7. Mixed-use development standards are in place to ensure that noise and vibration impacts are minimized.
PS 7.3	Strive to reduce the transportation noise on the I-15.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.

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PS 7.4	Consider estimated roadway noise contours based upon General Plan Figure 3.6 Noise Contours, when making land use decisions along busy roadways throughout the City.	CONSISTENT. See consistency discussion for Goal PS 7. The primary source of the General Plan Figure 3.6 Noise Contours that are within the Plan boundaries are due to traffic on Interstate 15. The properties most directly impacted by these noise contours are located outside of the Plan boundaries. Nevertheless, noise sensitive uses within the Plan area shall be protected by appropriate noise mitigation measures to ensure noise standards are not exceeded.
PS 7.5	Participate and cooperate with other agencies and jurisdictions in the development of noise abatement plans for highways.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
3.8.2	FIRE AND POLICE/LAW ENFORCEMENT GOALS AND POLICIES	
Goal PS 8	Provide efficient and effective public safety services for the community	CONSISTENT. Development within the Plan will contribute its fair share of impact fees for public safety services.
PS 8.1	Continue to follow Riverside County Fire Department most current guidelines to achieve standard response times and staffing levels.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, development within the Plan shall contribute its fair share of impact fees that include fees intended to offset increases in demand for fire services.
PS 8.2	Coordinate with the County of Riverside to provide adequate police services and staffing levels.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, increased activity in the downtown area is anticipated, and development within the Plan will contribute its fair share of impact fees that offset increases in demand for police protection services

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PS 8.3	Continue to provide Lake Patrol personnel who enforce boating rules and regulations, and perform rescue tactics.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, development within the Plan will contribute its fair share of impact fees that fund Lake Patrol protection services
PS 8.4	Promote the establishment of programs such as Neighborhood Watch and Crime-Free Multi-Housing in conjunction with law enforcement agencies to encourage community participation in the surveillance of neighborhoods.	CONSISTENT. Mixed-use and multi-housing projects shall be reviewed by law enforcement to assure that their design facilitates law enforcement surveillance of neighborhoods. The establishment of community participation programs such as Neighborhood Watch and Crime-Free Multi-Housing in new housing projects will be encouraged.
3.8.4	SCHOOLS GOALS AND POLICIES	
Goal PS 9	Encourage all school districts serving Lake Elsinore to provide school facilities that are adequate to service all students	CONSISTENT. A school facilities fee, which provides funding for school construction, was authorized by California Education Code Section 17620 and Government Code Section 65995 to mitigate the potential increased demand for school facilities associated with an increased number of residences. Residential development in the Plan is subject to fees associated with public school service based on size of residential units and as established by the school district. Present City policy requires that verification of payment of school fees be made prior to the issuance of building permits. Additionally, a portion of the property taxes generated by the project would be allocated to the school district.
PS 9.1	Encourage the establishment and development of a trade school, junior college,	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the

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	and/or four-year college campus within the City boundaries.	Plan. These education facilities may be considered in other areas of the City.
PS 9.2	Continue cooperation between school districts and the City to provide joint use of recreational facilities.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
3.8.6	LIBRARIES GOALS AND POLICIES	
Goal PS 10	Encourage the County of Riverside's County/City Public Library System to provide adequate library facilities for City residents.	CONSISTENT. Libraries are encouraged in three land use designations in the Plan. Furthermore, development in the Plan will be subject to development impact fees, which include those intended to fund new and expanded library facilities.
L	ANIMAL SERVICES GOALS AND POLICIES	
Goal PS 11	Provide high quality animal control services to ensure timely response and effective control that protect both citizens and animals.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
PS 11.1	Continue to foster and participate in the operation of a regional animal control facility through participation in the South Western Communities Financing Authority.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
PS 11.2	Continue to develop an educational program in conjunction with Animal Friends of the Valley regarding animal control services including spay and neuter programs.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, developers will be asked to cooperate with the City in distributing information regarding animal control services to future residents.

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3.8.10	UTILITIES GOALS AND POLICIES	
Goal PS 12	Ensure that adequate electrical, natural gas and telecommunications systems are provided to meet the demand of new and existing development.	CONSISTENT. Land use proposals in the Plan will be conditioned for the provision of all utilities to their site. Development will be subject to applicable service connection and development impact fees, which will pay for infrastructure and services that support development.
PS 12.1	Coordinate with the utility agencies to provide for the continued maintenance, development and expansion of electricity, natural gas, and telecommunications systems to service residents and businesses.	CONSISTENT. Land use proposals in the Plan will be conditioned for the provision of all utilities to their site.
PS 12.2	Encourage developers to contact Southern California Edison early in their planning process, especially for large-scale residential or non-residential development or specific plans, to ensure the projected electric loads for these projects are factored into SCE's load forecasts for the community.	CONSISTENT. The Plan document will contain recommendations for development including coordination with utility agencies serving the site.
PS12.3	Encourage developers to incorporate energy efficient design measures into their projects and pursue available energy efficiency assistance programs from SCE and other utility agencies.	CONSISTENT. Development projects in the Plan will be subject to land use planning review, including the efficient use of energy.

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3.8.12	TRASH AND RECYCLING GOALS AND POLICIES	
Goal PS 13	Encourage the City's franchise trash hauler(s) to provide and expand service for the collection, storage, transportation, recovery, and disposal of solid waste to meet the needs of the City.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
PS 13.1	Request the City's franchise trash hauler(s) to establish long-term solid waste management plans that include goals for recycling and source reduction programs.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
PS 13.2	Request that the City's franchise trash hauler(s) provide a public education program in recycling and source reduction techniques for homes, businesses, and construction.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, developers will be encouraged to cooperate with the City in distributing information regarding recycling and source reduction techniques affecting construction, residential and business users.
3.8.14	TELECOMMUNICATIONS GOALS AND POLICIES	
Goal PS 14	Encourage the pursuit of state of the art Information Technology	CONSISTENT. Development in the Plan will be encouraged to provide telecommunications access in residences and buildings.
PS 14.1	Encourage the use of information technology as a communication tool to improve personal convenience, reduce dependency on non-renewable resources, take advantage of ecological and	CONSISTENT. Development in the Plan will be encouraged to provide telecommunications access in residences and buildings.

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	financial efficiencies of new technologies.	
PS 14.2	Maintain and update the City's website with information about current events and issues, key leadership figures, community involvement opportunities, and educational tools such as solid waste management techniques and emergency preparedness programs.	NOT APPLICABLE. This policy provides direction to the City. The City's website maintenance procedures are already established. This policy does not apply to the Plan. However, development in the Plan, particularly those that provide recreational events, shall be encouraged to promote their activity schedules and job applications on the City's website.
Chapter 4	RESOURCE PROTECTION AND PRESERVATION	
4.2.5	BIOLOGICAL RESOURCES GOALS AND POLICIES	
Goal RP 1	Identify and conserve important biological habitats where feasible while balancing the economic growth and private property right interests of the City, its residents, and landowners.	CONSISTENT. The majority of the Plan area is not located within an MSHCP Criteria Cell. Only a portion of the Downtown Recreation designated area adjacent to the Lake is located in a Criteria Cell. It is not anticipated that development in accordance with the Plan will adversely affect important biological habitats.
RP 1.1	The City shall continue to participate in the Western Riverside County Multiple Species Habitat Conservation Plan, the LEAPS program, and the Implementing Agreement; with a strategy that focuses on quality assemblage of conservation acreage. The City shall work toward the lower end of the conservation acreage range	CONSISTENT. Development in the Plan is subject to MSHCP fees, pursuant to City requirements.

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	as promised by the County during the adoption of the MSHCP by the City.	
RP 1.2	Evaluate the installation of barrier fencing or other buffers between MSHCP Conservation Areas and proposed public and private land uses that may be incompatible with the Conservation Areas in order to minimize illegal/unauthorized public access, domestic animal predation, or dumping in the Conservation Areas while not impeding wildlife movement.	CONSISTENT. No MSHCP Conservation Areas exist in the Plan; although the Regional Conservation Authority (RCA) identifies the Outlet Channel (designated by the Plan as Floodway) and the City-owned properties in the Downtown Recreational designation as “Public/Quasi-Public Conserved Lands”.
RP 1.3	The City’s Conceptual Reserve Design shall be developed in accordance with Section 3.2.3 of the MSHCP as amended, and may rely upon the flexibility permitted by the MSHCP where appropriate in conducting the Reserve Assembly Accounting set for in Section 6.7 of the MSHCP.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
RP 1.4	Encourage revegetation with native plants compatible with natural surrounding habitat where soils have been disturbed during construction, and discourage plants identified	CONSISTENT. The Plan is not located adjacent to natural surrounding habitat; but still provides landscaping guidelines that require the use of drought-tolerant, non-invasive plants.

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	in the MSHCP as unsuitable for conservation areas.	
RP 1.5	The City shall coordinate with the Regional Conservation Authority to have that agency acquire native habitat areas as permanent open space and allow public trail access where appropriate.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
RP 1.6	The City shall establish a plan for a trail network intended for active or passive use within public open space areas and traversing around and through MSHCP Conservation areas where compatible with guidelines set forth in the MSHCP and City Council MSHCP policies.	CONSISTENT. The Plan incorporates a portion of the trail, sidewalk, and other pedestrian circulation systems that provide connectivity to MSHCP Conservation areas along Lake Elsinore.
RP 1.7	The City shall require all new trails, trailheads, conservation signage, interpretive centers, and maintenance facilities established within MSHCP Conservation areas to follow the Guidelines for the Siting and Design of Trails and Facilities, as set forth in Section 7.4.2 of the MSHCP.	CONSISTENT. No MSHCP Conservation Areas exist in the Plan; although the Regional Conservation Authority (RCA) identifies the Outlet Channel (designated by the Plan as Floodway) and the City-owned properties in the Downtown Recreational designation as "Public/Quasi-Public Conserved Lands". Any new trail facilities within these areas will be consistent with Section 7.4.2 of the MSHCP.
RP 1.8	The City shall consult with the Regional Conservation Authority (RCA) and adjacent jurisdictions to	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. The bikeway and pedestrian trail systems provide multiple interconnections

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	ensure proper adherence to MSHCP guidelines and to allow for a maximum level of regional interconnection of trails systems. The City shall modify or add to the regional interconnections and linkages based on new biological analysis brought forward during the CEQA and LEAP processes.	with other trails in the City, and is consistent with the City's Trails and Bikeways Plan.
Goal RP 2	Protect sensitive plant and wildlife species residing or occurring within the City.	CONSISTENT. The majority of the Plan area is not located within an MSHCP Criteria Cell. Only a portion of the Downtown Recreation designated area adjacent to the Lake is located in a Criteria Cell. It is not anticipated that development in accordance with the Plan will adversely affect habitats with sensitive plant and wildlife species.
RP 2.1	Biological resources analyses of proposed projects shall include discussion of potential impacts to any plant or wildlife species that is officially listed as threatened or endangered by the United States Fish and Wildlife Service and/or the California Department of Fish and Game but not covered by the MSHCP.	CONSISTENT. No plant or wildlife species that is officially listed as threatened or endangered by the United States Fish and Wildlife Service and/or the California Department of Fish and Game, but not covered by the MSHCP, have been identified within the Plan.
RP 2.2	Development or modification shall be discouraged in areas containing riparian habitat	NOT APPLICABLE. No known habitats were identified within the Plan.

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	<p>of high functions and values or corridors with 80% or more of natural native habitat that link larger patches of natural native habitat containing 80% or more native plant species. Further, development in areas described for conservation, including areas planned for riparian/riverine restoration included in the MSHCP, shall also be discouraged</p>	
RP 2.3	<p>The City shall encourage the development of a Native Tree Planting and Maintenance Program that presents guidelines for selecting and locating trees to support wildlife, improve air and water quality, and reduce energy consumption.</p>	<p>NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. However, the Plan supports trees that sequester carbon and provide shade, thereby reducing energy consumption.</p>

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4.3.4	OPEN SPACE GOALS AND POLICIES	
Goal RP 3	Provide an open space layout within the City that will enhance the recreational and visual experiences of all City residents and visitors.	CONSISTENT. The Plan provides recreational and passive open space that will enhance the recreational experiences of future residents and visitors. Additionally, the Plan intends to form strong visual and physical relationship with the Lake, an important environmental resource to the City, through the creation of view corridors and locating public areas adjacent to the Lake. The Plan incorporates area dedicated to the continuation of the Lake Elsinore Regional Trail, which will connect directly to the internal on-site pedestrian circulation.
RP 3.1	Maximize the MSHCP conservation areas and other open space that is available for public use.	CONSISTENT. See consistency discussion for Goal RP 3.
RP 3.2	The City shall ensure that passive and active open space uses are incorporated into development areas.	CONSISTENT. The Plan provides for development that incorporates active and passive open space with a wide range of recreational facilities.
RP 3.3	Development on steep slopes in public or private property shall require contour grading.	CONSISTENT. No grading on steep slopes is anticipated in the Plan. However, should contour grading be required, development shall provide contour grading as required through the City's development review process.
RP 3.4	Preserve the City's visual character, in particular the surrounding hillsides, which topographically define the Lake region.	CONSISTENT. The Plan's development standards preserve the visual character and views of the Lake where feasible.

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4.4.3	WATER RESOURCES GOALS AND POLICIES	
Goal RP 4	Improve water quality and ensure the water supply is not degraded as a result of urbanization of the City.	CONSISTENT. The Plan provides standards that aid in water conservation and the prevention of water pollution. Development will be required to employ best management practices (BMPs) to protect water quality. Additionally, the Plan calls for drought-tolerant landscaping and other water conserving measures to reduce the water demand associated with development.
RP 4.1	Encourage developers to provide clean water systems that reduce pollutants being discharged into the drainage system to the maximum extent feasible and meet required federal National Pollutant Discharge Elimination System (NPDES) standards.	CONSISTENT. Prior to the approval of a development project in the Plan, a Water Quality Management Plan shall be designed, submitted for review and approval, to address pollutant discharge generated by the project.
RP 4.2	Support public education and awareness programs to reduce pollutant discharges into the drainage system.	CONSISTENT. Developers in the Plan will cooperate with the City to distribute education materials when available. The development's WQMP will incorporate educational materials regarding water quality that will be provided to commercial tenants and residential owners.
RP 4.3	Require Best Management Practices through project conditions of approval for development to meet the federal NPDES permit requirements.	CONSISTENT. Developers in the Plan will be required to employ best management practices (BMPs) to protect water quality.

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RP 4.4	The City shall utilize the 1998 North American Vertical Datum to be consistent with the national standard for mean sea level, which would increase the measurement of the mean sea level for Lake Elsinore by approximately 2.4 feet.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
4.5.3	MINERAL RESOURCES GOALS AND POLICIES	
Goal RP 5	Balance the importance of conserving mineral resource areas that have been determined to be significant, the need for extracted materials for local construction, and the potential impacts and conflicts that may result.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. No known mineral resources are located in the Plan.
RP 5.1	The City shall consider the public benefits in allowing extraction activities of mineral resources when making land use decisions.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
RP 5.2	The City shall require mined property to be left in a condition suitable for reuse in conformance with the General Plan land use designations and the California Surface Mining and Reclamation Act (SMARA).	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan. No known mineral resources are located in the Plan.
RP 5.3	The City shall encourage the reuse and recycling of existing aggregate	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	construction material for new residential, commercial and industrial development.	
4.6.8	CULTURAL AND PALEONTOLOGICAL RESOURCES GOALS AND POLICIES	
Goal RP 6	Preserve, protect, and promote the cultural heritage of the City and surrounding region for the education and enjoyment of all City residents and visitors, as well as for the advancement of historical and archeological knowledge.	CONSISTENT. Development in the Plan is subject to the provisions of CEQA. Standard mitigation measures that preserve, protect and promote the City's cultural heritage in an appropriate manner, will be applied to new development projects within the Plan's boundaries.
RP 6.1	Encourage the preservation of significant archaeological, historical, and other cultural resources located within the City.	CONSISTENT. See the Response to Goal RP-6.
RP 6.2	The City shall consult with the appropriate Native American tribes for projects identified under SB 18 (Traditional Tribal Cultural Places).	CONSISTENT. SB 18 applies to all specific plans and specific plan amendments. In accordance with the City's procedures, the City notified Native American tribes as directed by SB 18. The City has consulted with those tribes that have requested consultation.
RP 6.3	When significant cultural/archeological sites or artifacts are discovered on a site, coordination with professional archeologists, relevant state and, if applicable, federal agencies, and the appropriate Native American tribes regarding	CONSISTENT. Development projects within the Plan will be required to comply with this policy.

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	<p>preservation of sites or professional retrieval and preservation of artifacts or by other means of protection, prior to development of the site shall be required. Because ceremonial items and items of cultural patrimony reflect traditional religious beliefs and practices, developers shall waive any and all claims to ownership and agree to return all Native American ceremonial items and items of cultural patrimony that may be found on a project site to the appropriate tribe for treatment. It is understood by all parties that unless otherwise required by law, the site of any reburial of Native American human remains or cultural artifacts shall not be disclosed and shall not be governed by public disclosure requirements of the California Public Records Act.</p>	
RP 6.4	<p>If archeological excavations are recommended on a project site, the City shall require that all such investigations include Native American consultation, which shall</p>	<p>CONSISTENT. Development projects within the Plan will be conditioned to comply with this policy.</p>

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	occur prior to project approval.	
Goal RP 7	Support state-of-the-art research designs and analytical approaches to archeological and cultural resource investigations while also acknowledging the traditional knowledge and experience of the Native American tribes regarding Native American culture.	CONSISTENT. Latest investigation and analysis techniques consistent with City policies are considered when reviewing resource assessments.
RP 7.1	Consult with California Native American tribes prior to decision-making processes for the purpose of preserving cultural places located on land within the City's jurisdiction that may be affected by the proposed plan. In accordance with State or Federal requirements.	CONSISTENT. The City consulted with the applicable California Native American tribes who requested consultation consistent with Senate Bill 18 and Assembly Bill 52. These consultations occurred prior to approval of the Plan.
RP 7.2	Continue to identify, document, evaluate, designate, and preserve the cultural resources in the City.	CONSISTENT. See response to Goal RP 6.
RP 7.3	Continue to update a citywide inventory of cultural resources in conformance with state standards and procedures while maintaining the confidentiality of	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	information as required by law.	
RP 7.4	Support the permanent curation of archaeological artifact collections by universities or museums or appropriate tribal facilities.	CONSISTENT. Development projects within the Plan shall be required to comply with this policy.
RP 7.5	Increase opportunities for cultural heritage tourism by promoting the history of Lake Elsinore to attract cultural heritage travelers while maintaining the confidentiality of Native American sites, places and other information as required by law.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
Goal RP 8	Preserve paleontological resources occurring within the City.	CONSISTENT. General Plan Figure 4.6 (Paleontological Resources) shows the Plan area as having a “Low” potential sensitivity for paleontological resources. Nevertheless, standard mitigation measures that protect paleontological resources that are found during construction will be applied to new development projects within the Plan’s boundaries.
RP 8.1	For development in areas delineated as “High” or “Undetermined” potential sensitivity for paleontological resources, require the project applicant to hire a certified paleontologist, who must perform a literature search and/or survey and apply the relevant treatment for the	NOT APPLICABLE. General Plan Figure 4.6 (Paleontological Resources) shows the Plan area as having a “Low” potential sensitivity for paleontological resources.

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	site as recommended by the Society for Vertebrate Paleontology.	
4.7.3	HISTORICAL PRESERVATION GOALS AND POLICIES	
Goal RP 9	Assure the recognition of the City's heritage through preservation of the City's significant historical sites and structures.	CONSISTENT. The Plan identifies significant historical sites and encourages preservation in accordance with the guidelines in the General Plan.
RP 9.1	Require the developer to obtain a professional, qualified historian to conduct a literature search and/or survey for any project that entails demolition or modification of an existing structure that may be of historical value in relation to the City's cultural heritage.	CONSISTENT. This General Plan policy is implemented by a requirement within the Architecture section of the Plan.
RP 9.2	Apply the General Plan "Historic Elsinore Design Standards" to the Lake Elsinore historic district, as defined in the City zoning ordinance.	CONSISTENT. The Plan has specified standards and guidelines that simplify and replace the outdated Historic Elsinore Architectural Design Standards originally adopted in 1993.
RP 9.3	Work with the Lake Elsinore Historical Society to create and periodically update a historic register of structures and other landmarks valuable to the cultural heritage to the City.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan.
RP 9.4	Where historic structures that do not possess a meaningful association with	CONSISTENT. The Plan supports efforts to preserve historic structures, including relocation to more appropriate sites.

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	the immediate surroundings are identified within the City, the City shall consider allowing relocation of the structure to an appropriate site.	
RP 10	Encourage the preservation, protection, and restoration of historical and cultural resources.	CONSISTENT. The Plan includes design standards for the downtown. The Plan encourages the preservation or restoration of historical buildings.
RP 10.1	Continue to implement the Historic Preservation Guidelines that guide historic preservation efforts as set forth in the Historic Elsinore Design Guidelines and the Downtown Master Plan.	CONSISTENT. The Plan includes design standards for the downtown. The Plan encourages the preservation or restoration of historical buildings.
RP 10.2	Integrate historic and cultural resources in land use planning processes where feasible to avoid conflict between the preservation of historic resources and alternative land uses.	CONSISTENT. The Plan includes design standards for the downtown. The Plan encourages the preservation or restoration of historical buildings.
RP 10.3	All City-owned sites designated as historical resources should be maintained in a manner that is consistent with the U. S. Secretary of the Interior's Standards for the Treatment of Historic Properties.	CONSISTENT. The City follows the standards for the treatment of historic buildings, for both City-owned sites and privately owned buildings.
RP 10.4	Encourage owners of historic resources to utilize	CONSISTENT. During the processing of design review applications, Planning staff

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	federal incentives including Federal Rehabilitation Tax Credits, facade and conservation easements, and to coordinate with the State Historic Preservation Office.	reviews and assists where appropriate and feasible the preservation of historic buildings.
4.8.3	AESTHETIC GOALS AND POLICIES	
Goal RP 11	Provide and maintain a natural and built environment that is visually pleasing to City residents and visitors.	CONSISTENT. The Plan provides standards and guidelines for attractive development. Development is encouraged to form strong visual and physical relationships with the Lake and mountains through the creation of view corridors and locating public areas adjacent to the Lake.
RP 11.1	For new developments and redevelopment, encourage the maintenance and incorporation of existing mature trees and other substantial vegetation on the site, whether naturally-occurring or planted, into the landscape design.	CONSISTENT. Where appropriate and feasible from biological and financial standpoints, existing mature trees and other substantial vegetation located on-site will be preserved through replanting or in-situ.
RP 11.2	Maintain and improve the quality of existing landscaping in parkways, parks, civic facilities, rights-of-ways, and other public open areas.	CONSISTENT. Existing landscaping in parkways and parks will be maintained and improved.
RP 11.3	Where appropriate, encourage new planting of native and/or non-invasive ornamental plants to enhance the scenic setting of public and private lands.	CONSISTENT. The Plan provides landscaping guidelines that require the use of drought-tolerant, non-invasive plants. Landscape designs will also include energy efficient irrigation.

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RP 11.4	Incorporate the City's identification symbol into street signage, planters, benches, public buildings, City vehicles, streetscape furnishings, and other appropriate applications.	CONSISTENT. The Plan requires that where appropriate and feasible, signage or monumentation will include the City's "E" logo, the "City of Lake Elsinore Dream Extreme" banner, or other City of Lake Elsinore branding consistent with the City's approved Identity Standards Guide
RP 11.5	Support a high level of Code Enforcement to encourage neighborhood beautification and to maintain property values and quality of life.	NOT APPLICABLE. The policy directs the City to fund Code Enforcement. Development within the Plan shall follow standards that beautify the area.
RP 11.6	Coordinate with agencies to screen, landscape and otherwise obscure or integrate public utility facilities, including electric power substations, domestic water and irrigation wells, switching and control facilities.	CONSISTENT. Developers in the Plan shall cooperate with the City in its efforts to meet this objective.
RP 11.7	Promote and facilitate the placement of public art that creates a unique setting and enhances a cultural and aesthetic character throughout the City.	CONSISTENT. The Plan includes policies regarding public art. The Plan also proposes an Artwork Program wherein contribution funding will be used to acquire and display public art.
Goal RP 12	Preserve valued public views throughout the City.	CONSISTENT. The Plan will form strong visual and physical relationships with the Lake and mountains through the creation of view corridors and locating public areas adjacent to the Lake.
RP 12.1	Encourage development designs and concepts that provide public views of Lake Elsinore and local	CONSISTENT. See consistency discussion for Goal RP 12.

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	ridgelines, through proper siting, building design and landscape design.	
RP 12.2	Encourage the dedication of open space in hillside development proposals to preserve and enhance view opportunities from transportation corridors and surrounding development.	CONSISTENT. Although the Plan area is relatively flat, dedication of open space is encouraged, and view corridors to the Lake are promoted as essential links to this valued City resource.
RP 12.3	Encourage new development and redevelopment to incorporate views of Lake Elsinore from roadways and other public spaces that provide residents and tourists with scenic vistas to the water, marinas, and lakeshore activities.	CONSISTENT. Land uses within the Plan area are encouraged to incorporate rooftop uses that will take advantage of potential views of Lake Elsinore.
RP 12.4	Establish a series of community gateways and entry statements to promote the visual character of the Districts.	CONSISTENT. The Plan has refined its boundaries and has located a “Gateway to the Downtown” entry monumentation designed at the intersection of Flint and Main Streets.
RP 12.5	Consider petitioning Caltrans to take control of portions of the SR-74 corridor, in order to promote signage and landscaping that enhance and preserve the corridor’s aesthetic setting.	NOT APPLICABLE. This policy provides direction to the City; it does not apply to the Plan because the SR-74 is not within nor adjacent to it.
Goal RP 13	Minimize activities, development, and landform modification that could	CONSISTENT. Development standards within the Plan were included to improve the City’s visual character.

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	distract viewers from the City's visual character.	
RP 13.1	Discourage extractive activity from being conducted in highly visible areas and require reclamation of these mining areas. If such uses must occur in visible areas, the City shall require extensive visual screening with landscaping and/or fencing.	NOT APPLICABLE. No mineral resources are known to exist within the boundaries of the Plan; nor are extractive activities proposed or permitted within the Plan area. Grading may require some excavation; however, the existing topography will not be significantly modified by development in the Plan.
RP 13.2	Discourage extractive uses or development that entails excessive light and glare visible from private and public viewpoints.	CONSISTENT. No extractive activities are proposed or permitted within the boundaries of the Plan. Attention to night lighting of other development activities shall be required so that excessive light and glare are shielded or otherwise minimized.
RP 13.3	Require grading plans for any hillside development to include specifications for revegetation and new planting to minimize hillside scarring.	CONSISTENT. Hillside grading is not anticipated to occur in the Plan. However, revegetation will be required where necessary following any development-related grading.
4.9.2	SUSTAINABLE ENVIRONMENT GOALS AND POLICIES	
RP Goal 14	Reduce greenhouse gas emissions from all activities within the City boundaries to support the State's efforts under AB-32 and to mitigate the impact of climate change on the City, State, and world.	CONSISTENT. The Plan development standards include energy conservation guidelines that will improve energy efficiency in development projects, which will also result in fewer greenhouse gases emissions.
RP 14.1	By 2020, the City will reduce greenhouse gas emissions from within its boundaries	NOT APPLICABLE. This policy directs the City to establish targets for reduction of

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	to 1990-levels consistent with AB 32.	greenhouse gas emissions; it does not apply to the Plan. See the discussion in RP Goal 14.
RP 14.2	Measures shall be established that aim to reduce emissions generated from City uses, community uses (community actions) and new development (City discretionary actions).	NOT APPLICABLE. This policy directs the City, and does not directly apply to the Plan. However, the City has approved a Climate Action Plan, which provides thresholds applicable to all land uses, including those within the Plan boundaries.
RP 14.3	The City shall strive to increase public awareness of climate change and climate protection challenges.	NOT APPLICABLE. This policy directs the City; it does not apply to the Plan.
RP 14.4	The City will participate in the Sustainable Communities Strategy/Regional Blueprint Planning effort to ensure that local plans are consistent with the Regional Plan.	NOT APPLICABLE. This policy directs the City; it does not apply to the Plan.
HISTORIC DISTRICT PLAN		
6.3.3	OVERALL DISTRICT GOAL	
Goal 1	The primary goal of the Historic District is to create a true nucleus of the City. Which represents the physical and symbolic heart and the civic, commercial, and cultural center, highlighted by its connection to the lake. Goal 4 of the General Plan's Community Form (Chapter 2.0) preserves the rich historical heritage of the	CONSISTENT. The Plan creates the appropriate land use designations as a framework for civic, commercial, cultural as well as residential development. The Plan seeks to provide opportunities for a walkable community, maintaining the rich heritage of historic buildings, and promoting both residential and tourist services, in a vibrant Main Street setting. The Plan includes a Downtown Recreational designation adjacent to the Lake, to capitalize on its beauty and incentives for water-related activities.

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	City of Lake Elsinore by maintaining and ensuring the continuation of the Historic District area as the premier civic, commercial, and cultural center.	
6.4.1	URBAN DESIGN GOALS AND POLICIES	
Goal 2	Support mixed-use corridors with a pedestrian orientation towards the public realm, enhance public views of the lake and Santa Ana Mountains by establishing view corridors with appropriate development standards, and capture new public views where possible as lakefront sites are redeveloped.	CONSISTENT. The Plan includes predominantly a mixed-use land use designation, particularly on both sides of Main Street. Design Standards for both residential and commercial uses encourage the development of a walkable environment. The Public Realm is intended to include public art, pedestrian attractions, and view corridors to the Lake.
HD 2.1	Through the project and CEQA processes, create a strong connection between Main Street and the public/institutional uses and commercial uses north and south of Lakeshore Drive.	CONSISTENT. The Downtown Recreational land use designated adjacent to the Lake, provides a wide variety of uses that attract both residents and tourists from Main Street to Lakeshore Drive.
HD 2.2	All Historic District development shall adhere to the design guidelines established through the Downtown Master Plan, and the 1994 Historic Elsinore Architectural Design Standards.	CONSISTENT. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation. The Plan's architectural requirements supersede all previously adopted architectural design

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
		standards for the Downtown Elsinore Specific Plan area.
HD 2.3	Through the project and CEQA processes, ensure an attractive destination with a diversity of uses and activities while offering pedestrian safety and mobility.	CONSISTENT. Each development project within the Plan shall comply with the Plan land use matrix, and shall meet the development standards, design guidelines, and Bike and Hiking Trails standards in order to achieve this policy.
HD 2. 4	Encourage the concept of commercial mixed use along Main Street.	CONSISTENT. The Plan predominantly provides mixed-use land uses, particularly along both sides of Main Street, in order to make it a “great street,” with vibrant nightlife and activities.
HD 2.5	Through the project and CEQA processes pursue the design and construction of community identification signs and gateway monuments at the main points of entry to the Historic District,	CONSISTENT. The Plan requires gateway monumentation for Historic Main Street at the intersection of Flint Avenue and Main Street, and recommends secondary monumentation at Lakeshore Drive if feasible.
HD 2.6	Through the project and CEQA processes improve roadways with additional trees, landscaping themes, widened and contiguous sidewalks, functional public furniture, additional decorative lighting, and maximum on-street diagonal parking.	CONSISTENT. The Plan identifies street furniture, lighting and landscaping as important elements of the public realm. The Plan calls for a walkable neighborhood, therefore contiguous sidewalks will be required by development. Strategically placed parking structures may prove more efficient to support the “park-once-and-walk” concept for the downtown.
HD 2.7	Strongly encourage the use of architectural designs that are “true to form” with the architectural designs	CONSISTENT. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	selected and allowed by the Downtown Master Plan and the Historic Elsinore Architectural Design Standards.	Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation. The Plan's architectural requirements supersede all previously adopted architectural design standards for the Downtown Elsinore Specific Plan area.
HD 2.8	Through the project and CEQA processes create an integrated and memorable relationship of architecture, public space, and open space in developments immediately adjacent to the Temescal Wash outflow channel, the future Civic Center, and recreational areas.	CONSISTENT. The Plan identifies the existing Riverwalk trails, adjacent to the Temescal Wash Outlet Channel, as an important feature of the downtown area. The mixed-use designation along its border encourages integrated development of residential and commercial uses.
HD 2.9	Pursue the development of a new Civic Center complex within proximity to Main Street and the lake. Public and cultural facilities such as City Hall, an open-air public plaza, and a theatre shall be encouraged.	CONSISTENT. The Plan identifies civic and cultural uses within the downtown area, particularly theatre and entertainment venues. Additionally, the Plan calls for a public realm or plaza, signage and artwork that promotes the City's heritage. "Government building & facility" land uses are permitted in all of the Plan's land use designations, with the exception of the Open Space and Floodway designations.
HD 2.10	Pursue the preparation and adoption of a Downtown Master Plan which follows the boundaries of the Downtown Overlay area.	CONSISTENT. The Main Street Overlay boundaries, as shown on the General Plan's Land Use Plan (Figure 2.1A) and Historic District Land Use Plan (Figure HD-1), were used as the boundaries for the Downtown Master Plan that was adopted concurrently with the General Plan in 2011. The Downtown Elsinore Specific Plan's

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
		boundaries are identical to those of the Main Street Overlay, with the exception of that portion between Flint Avenue and Interstate 15.
6.5.1	HISTORIC PRESERVATION GOAL AND POLICIES	
Goal 3	Continue to restore and enhance the historical structures within the Historic District as development occurs. Maintain the history of the Historic District through restoration and expansion of existing historical structures. Educate and promote the significance of the Historic District's structures.	CONSISTENT. Chapter 2 (Context) of the Plan includes a summary of the City's history from its earliest times to the present. Table 2-1 is a listing of Historic Structures within the Downtown Elsinore Specific Plan and Vicinity. The Plan encourages the preservation or restoration of historical buildings.
HD 3.1	Support the expansion of the Lake Elsinore Historical Society's Museum located along Main Street.	CONSISTENT. The Plan allows museums in the "General Commercial", "Mixed Use", "Downtown Recreation" and "Public Institutional" land use designations. The current location of this Historical Society's museum is within the "Public Institutional" land use designation.
HD 3.2	Encourage new development to incorporate historical architectural features into building facades and require projects to conform/abide by the architectural design standards as defined in the Downtown Master Plan and the Historic Elsinore Architectural Design Standards.	CONSISTENT. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation. The Plan's architectural requirements supersede all previously adopted architectural design standards for the Downtown Elsinore Specific Plan area.

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HD 3.3	Encourage the preservation of all historic structures located within the Historic District, including those located outside the core downtown area.	CONSISTENT. The Plan encourages the preservation of historic structures.
HD 3.4	Consider the development and adoption of specialized design guidelines particular to the Historic District overlay.	CONSISTENT. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation.
6.6.1	TRANSPORTATION/CIRCULATION GOAL AND POLICIES	
Goal 4	Consider a circulation system that allows pedestrian connectivity throughout the Historic District with an emphasis on access to public spaces, recreational areas, and major roadways; along with developing an efficient circulation pattern with roadway standards that provide for higher traffic volumes parking demands in appropriate areas while maintaining a friendly pedestrian environment.	CONSISTENT. The Plan's goal is to create a walkable community, following a "park-once-and-walk" concept. The Plan includes predominantly a mixed-use land use designation, particularly on both sides of Main Street. Design Standards for both residential and commercial uses encourage the development of a walkable environment. The Public Realm is intended to include public art, streetscape, street furniture and pedestrian attractions.
HD 4.1	Continue efforts to complete the Channel Walk project.	CONSISTENT. The Plan supports the City's Trails Master Plan with the Riverwalk trail system along the Outlet Channel.
HD 4.2	Contemplate the design of new parking facilities as an	CONSISTENT. The Plan encourages the construction of strategically located parking

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	integral feature in the overall design of the Historic District.	structures that promote the “park-once-and-walk” concept for the downtown.
HD 4.3	In order to support pedestrian activity, mitigate traffic impacts to LOS E during peak hours.	CONSISTENT. The Plan’s goal is to create a walkable community within the downtown area.
HD 4.4	Traffic signals, if warranted, shall be appropriately designed for the downtown landscape.	CONSISTENT. The installation of traffic signals should be consistent with the public realm concepts contained within the Plan.
HD 4.5	The intersection of Graham Avenue and Main Street requires signalization according to the General Plan Traffic Study. Monitor the intersection and when the LOS falls below E, the City will prepare for signal installation.	CONSISTENT. Any future signalization of the Graham Avenue and Main Street intersection shall comply with the City’s standard traffic control requirements and procedures.
6.7.1	PARKS & RECREATION GOAL AND POLICIES	
Goal 5	Support a recreational system that is accessible to the City’s residents, employees, and visitors and ensuring a variety of facilities for all ages.	CONSISTENT. The Plan designates the area adjacent to the Lake as Downtown Recreational, which allows a wide range of commercial, public, recreational and civic uses in close proximity to the mixed-use areas to the east.
HD 5.1	Consider pedestrian linkages between the Channel Walk project and the nearby Historic District, commercial businesses, recreational facilities, major corridors, the Lake Edge Parkway, and the Lake.	CONSISTENT. The Plan strives for a walkable environment with development incorporating pedestrian paths, hiking trails and bicycle lanes. These facilities, including the Riverwalk along the Outlet Channel, in conjunction with sidewalks and on-site pedestrian circulation will create linkages with commercial businesses, recreational

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
		facilities, major corridors, the Lake Edge Parkway, and the Lake.
HD 5.2	Through the project and CEQA processes expand and support facilities within the various community centers.	CONSISTENT. The Plan includes community centers as an allowable land use in five of its eight land use designations.
HD 5.3	Encourage and pursue the development of additional recreational facilities adjacent to new public development such as the future civic center plaza.	CONSISTENT. The Plan allows recreation facilities in five of its eight land use designations (some areas require a conditional use permit).
HD 5.4	Encourage residential development to incorporate common open spaces on-site, such as courtyards and terraces.	CONSISTENT. The mixed-use designation allows higher density residential projects, which are required to provide common open spaces on-site.
6.8.1	HISTORIC DISTRICT OVERLAY POLICIES	
HD 6.1	Pursue the preparation and adoption of a Downtown Master Plan which follows the boundaries of the Main Street Overlay Special Treatment Area	CONSISTENT. The City adopted the Lake Elsinore Downtown Master Plan in 2011, which provided tools to revitalize the Main Street corridor. The Downtown Elsinore Specific Plan replaces the Downtown Master Plan; however, it retains the main focus to revitalize the downtown area as the premier civic, commercial and cultural destination of the City, and to provide a strong connection to the Lake. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation.

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
HD 6.2	Through the project review and CEQA processes, require new and renovated buildings to reinforce the stylistic characteristics of historic buildings in and around the vicinity of the Historic District. Use of building materials similar to those used during the area's historic era is required along facades facing Main Street.	CONSISTENT. The Downtown Elsinore Specific Plan requires that, in accordance with the City's General Plan, existing and new development will embody the rich historical heritage of the downtown. However, the Plan calls for an eclectic blend of both historic buildings and modern designs that reflect efficiency, innovation, and uniqueness. This flexibility of design encourages builders to utilize features that best accommodate their use, location and market base. Architectural expression and interpretation of design requirements will be proposed by the developer and approved through the City's Design Review process.
HD 6.3	Encourage underground parking or parking areas behind the development project, away from Main Street.	CONSISTENT. The Downtown Elsinore Specific Plan encourages underground parking, structures located along service streets, or behind the development project, away from Main Street.
HD 6.4	Consider density bonuses and reduced parking standards for residential and commercial developments when development provides various amenities including building architectural features, public art, public spaces, and open space areas.	CONSISTENT. Both density bonuses and lot consolidation incentives are available to developers within the boundaries of the Downtown Elsinore Specific Plan. The Plan reduces the amount of parking required by the Lake Elsinore Municipal Code in order to encourage the "park-once-and-walk" objective. Additionally, any land use developer may propose further reductions should a parking study prepared by a licensed engineer, and reviewed and approved by the City, recommend such reduction.
HD 6.5	Through the project review and CEQA processes ensure that new developments have a strong orientation	CONSISTENT. The Plan's Downtown Core, identified on both sides of Main Street between Heald and Library Streets, has a

Goal/ Policy #	Goal/Policy Text	Consistency Analysis
	towards Main Street and maintain a zero lot line along this roadway.	mixed-use designation, which allows for a zero front setback.
HD 6.6	Encourage the use of landscaping themes, public furniture, signage and lighting fixtures that add to the architectural character of historic structures and are consistent throughout Main Street.	CONSISTENT. The Plan's discussion of the "Public Realm" in Chapter 3, includes identification of street trees, street furniture and fixtures, and encourages the proliferation of public seating areas. Furthermore, the Plan supports Public Art and encourages an Artwork Program to capture funding from development.
HD 6.7	Through the project review and CEQA processes maintain and maximize public views of the Lake and the Santa Ana Mountains along Main Street. A maximum building height of four (4) stories shall be maintained to preserve view corridors to the Lake and the Santa Ana Mountains.	CONSISTENT. Due to the elevation of Main Street, ground-level views of the Lake are generally blocked along this corridor until you approach the Lake itself. For this reason, the Plan encourages rooftop uses that take advantage of and facilitate Lake and mountain views. Building heights are generally regulated by the floor area ratios of the Plan's land use designations.
HD 6.8	Consider revising the current Historic Elsinore Architectural Design Standards.	CONSISTENT. The Downtown Elsinore Specific Plan has incorporated much of the guideline concepts established in the former Downtown Master Plan and the Historic Elsinore Architectural Design Standards. The Downtown Elsinore Specific Plan updates and simplifies these guidelines for easy and flexible implementation. The Plan's architectural requirements supersede all previously adopted architectural design standards for the Downtown Elsinore Specific Plan area.

APPENDIX B

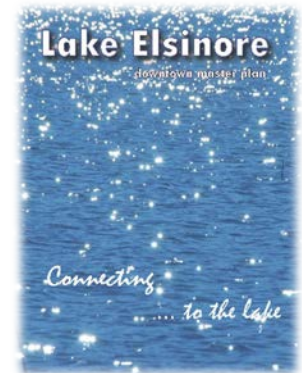
Community Outreach & Participation

APPENDIX B

Community Outreach & Participation

2011 Downtown Master Plan

During the development of the 2011 Downtown Master Plan, the City's public outreach engaged the community through open dialogue, consensus-building workshops and citizen surveys. The objective of this concerted effort was to create a Vision for the downtown area that responded to the citizens of Elsinore.



Documentation of the public outreach and participation that occurred between 2008 and 2011 during the development of the 2011 Downtown Master Plan was described on pages 27 through 33 of the Lake Elsinore Downtown Master Plan's "Connecting to the Lake" document. Copies of these pages are included in this Appendix.

2018 Downtown Elsinore Specific Plan

The Downtown Elsinore Specific Plan incorporates the Vision and many of the concepts developed during the City's 2008 through 2011 public outreach described above because it is a strong base from which to expand, update and simplify the Plan.

The City also included additional community outreach and participation during its preparation of the Downtown Elsinore Specific Plan. This includes meetings with groups that have a special interest in downtown Lake Elsinore and community workshops. The City mailed an invitation to attend the May 17, 2018 Community Workshop to over 426 residents, landowners, and adjacent property owners.

The City held the following meetings and workshops:

1. Historic Preservation Committee – February 22, 2018
2. Downtown Merchants Association – May 3, 2018
3. Community Workshop – May 17, 2018
4. Planning Commission Workshop – June 5, 2018

design objectives

the process

LAKE ELSINORE DOWNTOWN MASTER PLAN

The Lake Elsinore Redevelopment Agency engaged the services of the Cooper Carry team to undertake a comprehensive downtown master plan. The purpose of the downtown master plan is to identify the goals and opportunities for future development within Downtown Lake Elsinore. The purpose of the Downtown Master Plan was to set the guidelines for the future development as it relates not only to the City Hall / Civic Center but to the downtown as a whole.

One of the objectives of the Downtown Master Plan was to engage the community and citizens of Lake Elsinore in a consensus-building process. A series of public workshops were scheduled to determine the goals and objectives within the community of Lake Elsinore.

PURPOSE

The Lake Elsinore Downtown Master Plan provides a vision and strategic framework to guide the future development of its' downtown.

The purpose of this Master Plan Document:

- 1 - Identify the goals, objectives and desires of the community to create a vision and organizational framework for downtown Lake Elsinore;
- 2 - Create a master plan that is grounded in reality... a plan to build for generations to come.
- 3 - Develop a phasing strategy for implementation, through public and private partnerships;

PUBLIC OUTREACH

The public outreach process began in October 2008, and has been coordinated by Cooper Carry throughout the master planning process. From the outset, the RDA and the City of Lake Elsinore have embraced an open dialogue with the community and required that the master plan and its implementation respond directly to the needs and desires of the public and community stakeholders. The community outreach has continued throughout the process with stakeholder meetings, public workshops, small forum group meetings and a study session with City Council members and officials. In addition, the consultant team and City officials have engaged in regular working group meetings, to discuss specific details.

The goal of the public outreach is to gain input and support from the community about the future of their downtown. We recognize that the success of every downtown is all about the people who will use it on a daily basis, whether they live, work, shop, play or eat there.

On December 6, 2008, a public workshop was held to establish the goals and objectives for the downtown master plan. The workshop was divided into three parts, metaphorically based on important components of a traditional waterfront downtown: the Town Square, Main Street, and the Pier. The idea was to encourage maximum participation throughout. Convening at the Town Square to be introduced to the happenings downtown, participants would then take a stroll down Main Street to identify what would make downtown viable and livable. Ending at the Pier, they would consider ways to make downtown Lake Elsinore a truly memorable place. The following pages summarize the findings from the public workshops.

public workshops

THE MAIN STREET GAME

PUBLIC WORKSHOP SUMMARY

The City of Lake Elsinore, the Lake Elsinore Redevelopment Agency, and Cooper Carry, Inc. conducted a consensus-building workshop on December 6, 2008. The purpose of the workshop was to identify clear goals for the future of downtown Lake Elsinore. The workshop was divided into three parts, metaphorically based on important components of a traditional waterfront downtown: Town Square, Main Street, and Pier. The idea was to encourage maximum participation throughout. Convening at the Town Square to be introduced to the happenings downtown, participants would then take a stroll down Main Street to identify what would make downtown viable and livable. Ending at the Pier, they would consider ways to make downtown Lake Elsinore a truly memorable place.

town square The Town Square was the stage for a public forum. Participants were asked to respond to the important issues that have arisen from the General Plan update process and Civic Center Design Competition. Their comments, listed below, reemphasized some points already identified and brought new issues critical to the success of downtown Lake Elsinore to light.



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CONNECTIVE ARCHITECTURE

Connective Architecture strives to re-engage people to people, people to environment, and people to place.

How do we make Downtown Lake Elsinore Viable?

VIALE was defined as capable of working, functioning or developing adequately; having a reasonable chance at succeeding; financially stable; having the ability to grow, expand and develop.

How do we make Downtown Lake Elsinore Livable?

LIVABLE was defined as fit or pleasant to live in; suitable for living in; habitable; comfortable; worth living; enduring.

How do we make Downtown Lake Elsinore Memorable?

MEMORABLE was defined as noteworthy, significant, worth being remembered or noted; remarkable.



downtown master plan

public workshops

DOWNTOWN WORKSHOP COMMENTS

- bike paths and walking paths respectful to property owners – linear connective spine
- transportation water taxi or ferry across lake, bus system, trolley bus, etc.
- large public area at lake direct access to lake
- connection of boat launch and senior center along lakeshore
- signage
- flood prevention
- connectivity of downtown
- connective architecture connect 2 sides of lake?
- don't ruin nature/character of lake elsinore unique
- what is memorable about Lake Elsinore?
- safety in downtown no lights, sidewalks, stores open, cctv, more people
- no landscape around lake why isn't it cleaner/safer?
- parking on main street (City Hall)
- water features, windmills on the hills
- historic instead of "Old Town" references; there are places that don't have the same historic fabric as Lake Elsinore – "Faux" downtowns
- civic center as a part of downtown
- sidewalks by schools
- stronger stadium connectivity
- lake activities sail vs. power boats, fishing, no casino
- reuse of old buildings why are they not being reused? What are the issues, costs? Some buildings are eyesores and need more City involvement
- add social security office downtown
- civic center new roads, one-way streets, improved roads
- city park to stay deeded in perpetuity – water tower deeded to city
- seniors in downtown - better connection
- hot springs underground well – reuse within downtown
- new business park not on the lake but integrated into downtown
- boardwalk around lake private ownership issues – opportunities for connectivity

[connecting to the lake](#)



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public workshops

main street Leaving the Town Square, the workshop participants took a walk down Main Street. Six shop fronts (with tables behind) were set up along the theoretical street, three on either side of the street. Each shop represented a different topic important to creating a viable, livable, and memorable downtown. The color-coded quote cards determined at random the six discussion groups and the shop at which they would begin. The six topics were: Architectural Character, Civic, Commercial, Public Realm, Residential and The Waterfront.

ARCHITECTURAL CHARACTER

The influence of existing architecture on the form of future developments in downtown will begin to define an Architectural Character. Historic buildings in Lake Elsinore date from the late 19th century and were built to meet the functional and aesthetic needs of the City and its people. These needs may not be the same today, and the architecture must respond appropriately.

CIVIC

The Civic Center Design Competition presented the need for a new city hall, council chambers, post office, public library, business incubator, and other government offices in downtown. Instead of concentrating these uses into one Civic Center,

there is an opportunity to spread these uses along Main Street to help catalyze new investment and development around existing historic shops and other uses, while also addressing some of the vacancies and infrastructure needs in downtown. Traditionally, Civic buildings are the most prominent features of a downtown, embodying the spirit the community at large. They become the visual representation of where a city has been and where it is going.

COMMERCIAL

Commercial development is integral to establishing viability in downtown Lake Elsinore. The Proposed General Plan calls for commercial development along Main Street from Lakeshore Drive to the I-15 Freeway, a mile-long corridor. The feasibility of attracting viable commercial uses to cover the length of Main Street, especially in current market conditions, is questionable. A more focused hub of commercial activity may be a wiser strategy that ultimately contributes to a stronger downtown.

PUBLIC REALM

The Public Realm is the inter-connective fabric of downtown where people can gather and interact with other people and the environment. It is this interaction that begins to create the sense of place, shaping its unique character and ultimately making



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downtown master plan

public workshops

it memorable. The Public Realm includes sidewalks and crosswalks, parks, landscape and hardscape features, signage, lighting, awnings and shading structures, street furniture, piers, boardwalks, and other components that influence and inspire the pedestrian experience. Improvements within the public realm can change negative perceptions of downtown, enhance connections along Main Street from the I-15 Freeway to the lake, and make downtown a more pedestrian-friendly, vibrant, and memorable place where people want to be.

RESIDENTIAL

Integral to making downtown Lake Elsinore a safer and more active environment is creating more opportunities for people to live downtown. Currently, downtown consists primarily of commercial uses and land vacancies. When businesses and shops close at night, there are no “eyes on the street,” which can make one feel uncomfortable and unsafe. Encouraging people to live downtown can help to deter these negative perceptions. More people living downtown can make businesses more viable by creating local demand, extending the hours of shops and restaurants. Main Street can then become an inviting place to be from early in the morning until late in the evening.

WATERFRONT

The Waterfront is one of the most important components of downtown, providing an opportunity to make a visit to the City a memorable experience. Historically, the City developed around the quality of life supported by the lake, the largest of the handful in southern California that are natural. Today, downtown is disconnected from the lake. The Waterfront discussions sought to identify how to re-establish this important relationship. The Proposed General Plan allocates the waterfront area in downtown as Open Space/Recreation, which allows currently for open space and passive/active recreation, as well as commercial recreation facilities provided they exhibit exceptional architecture / site design / amenities.



connecting to the lake



public workshops

the pier The workshop culminated with a visioning session held on The Pier. An oversized board game was set up, with empty squares (similar to the property squares on a Monopoly board) forming two rows. Workshop participants were asked to identify the elements from the Town Square and Main Street discussions that they believed were most critical to making downtown viable, livable and memorable. These elements were then written in to the squares to fill out the board. Each participant was then given three fake ten-dollar bills. By placing money in the squares, they indicated where they wanted the City to invest money. The table opposite lists the elements, in order of dollars allocated to them.

top 5 dollar investments
pier (\$240)
street improvements (\$150)
streetlights, landscape, sidewalks
city/public-purchased land (\$120)
boardwalk (\$100)
city hall (\$80)
lake accessibility (\$70)



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downtown master plan

public workshops

additional dollar investments:

\$50
main street to lake connection
waterfront park

\$40
canal/channel riverwalk
ferry

\$30
library
performing arts center
p.r. / perception and marketing
home rehab grants

\$20
green initiatives
geothermal, sustainability
affordable housing
museum
post office
maintenance
lakeshore drive connection

\$10
sulphur springs
parking structure
law enforcement
entry archway
workforce training
ymca/boys and girls club
earthquake retrofit facades
grants
stoneman road crossing

\$0
water tower park
revenue/public boat slips
retrofit of main street public art
code enforcement
trolley



connecting to the lake

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APPENDIX C

Approval Documents

APPENDIX D

Mitigation Monitoring and Reporting Program

5.0 MITIGATION MONITORING AND REPORTING PROGRAM

5.1 INTRODUCTION

Pursuant to Section 21081.6 of the California Environmental Quality Act (California Public Resources Code, Sections 21000 et seq.) and Section 15097 of the State CEQA Guidelines (California Code of Regulations, Section 15000 et seq.), public agencies are required to adopt a mitigation monitoring and reporting program to ensure that the mitigation measures identified in an Environmental Impact Report are implemented. As stated in Section 21081.6(a)(1) of the Public Resources Code:

“The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation.”

The following table (Mitigation and Monitoring Program) provides the required information which includes the various mitigation measures set forth in the Recirculated Draft Program EIR for the proposed project, applicable implementation timing, identification of the agencies responsible for verifying implementation and the monitoring method for each identified mitigation measure.

The mitigation measures contain several acronyms that are defined in the RDP-EIR and Final Recirculated Program EIR, but may not be defined in the mitigation measures themselves. As used in the mitigation measures, these acronyms are defined as follows:

AQMD	Air Quality Management District
CDD	Community Development Director
CDFG	California Department of Fish and Game
CEQA	California Environmental Quality Act
EIR	Environmental Impact Report
GPU	General Plan Update
I-15	Interstate 15
MSHCP	Western Riverside County Multiple Species Habitat Conservation Plan
SARI	Santa Ana Regional Interceptor
SR-74	State Route 74

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5.2 MITIGATION MONITORING AND REPORTING PROGRAM

MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
3.1 Land Use and Planning				
MM Land Use 1: The Growth Management Program developed by the City provides a strategy for developing a pattern and rate of growth to ensure that adequate public facilities and infrastructure can be provided to meet the rate of new construction and population growth. The goals and policies under the Growth Management section of the Community Form chapter provide principles for a growth management section. Implementation of the development pattern provided in the Growth Management Program and implementation of policies from the Growth Management Section of the Community Form chapter, in association with future development, would reduce impacts related to the population and housing forecasts.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Land Use 2: Implementation of the GPU, the Land Use Plan, and District Plans could result in significant impacts related to disturbance of areas described for conservation in the MSHCP. Individual projects implemented pursuant to the Land Use Plan and District Plans in accordance with the Resource Protection and Preservation Chapter, Biological Resources Section, Goal 1, Policies 1.1-1.11 will be required to demonstrate their avoidance of significant impacts associated with areas described for conservation in the	Review of discretionary land use applications during City's development review process.	Determination of project consistency with MSHCP.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.

Section 5.0 - Mitigation Monitoring and Reporting Program



MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
MSHCP Conservation Areas. Future projects may be allowed to alter the Conservation Area boundaries through criteria refinement, minor amendments, or other means, but would be required to do so in conformance with all regulations and mitigation requirements of the MSHCP.				
MM Land Use 3: Each project within the Skylark Airport Influence Area, as shown on Figure 2.7 of the General Plan, will be reviewed for its consistency with the Airport Land Use Planning Handbook Recommendations when individual projects are proposed. This review will include analysis and subsequent review under CEQA. The feasibility of the proposed mitigation measures must be determined on a project-specific level.	Project-specific CEQA environmental analysis and the incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of individual projects in East Lake District.
MM Land Use 4: If the motocross track is relocated, future development within the East Lake District Plan shall be required to comply with mitigation measures identified in the East Lake Specific Plan EIR. However, additional project-specific CEQA environmental analysis and review will be required when a detailed project is proposed at the new motocross site. This project-level review will include an analysis of potential land use compatibility issues.	Project-specific CEQA environmental analysis and the incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of individual projects in East Lake District.
Implementation of mitigation measures MM Land Use 1 through MM Land Use 4 is required.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
3.2 Cultural and Paleontological Resources				

MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
MM Cultural/Paleontological Resources 1: Individual projects implemented in accordance with the Land Use Plan shall also demonstrate compliance with Land Use Policies 4.1-4.4, Cultural and Paleontological Resources Policy 6.1, and Historic Preservation Policies 9.1-9.4, and 10.1-10.4. As well as compliance with applicable District Plan Policies related to cultural and paleontological resources.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Cultural/Paleontological Resources 2: Prior to issuance of grading permit(s) for the project, the project applicant shall retain an archaeological monitor to monitor all ground-disturbing activities in an effort to identify any unknown archaeological resources. Any newly discovered cultural resource deposits shall be subject to a cultural resources evaluation.	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which requires that an applicant-retained qualified archaeologist monitor all ground disturbing activities and to submit summary report.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications. Prior to issuance of grading permit
MM Cultural/Paleontological Resources 3: At least 30 days prior to seeking a grading permit, the project applicant shall contact the appropriate tribe to notify that Tribe of grading, excavation and the monitoring program, and to coordinate with the City of Lake Elsinore and the Tribe to develop a Cultural Resources Treatment and Monitoring Agreement. The Agreement shall address the treatment of known cultural resources, the designation, responsibilities, and participation of Native American Tribal monitors during grading,	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which requires the submittal of an executed Cultural Resources Treatment and Monitoring	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications.
		Review and approval of	Community	At least 30 days prior to

Section 5.0 - Mitigation Monitoring and Reporting Program



MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
excavation and ground disturbing activities; project grading and development scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site.	Agreement at least 30 days prior to seeking a grading permit.	Cultural Resources Treatment and Monitoring Agreement.	Development Department - Planning Division Public Works Department - Engineering Division	issuance of a grading permit.
MM Cultural/Paleontological Resources 4: Prior to issuance of any grading permit, the project archaeologist shall file a pre-grading report with the City and County (if required) to document the proposed methodology for grading activity observation. Said methodology shall include the requirement for a qualified archaeological monitor to be present and to have the authority to stop and redirect grading activities. In accordance with the agreement required in MM Cultural/Paleontological Resources 2, the archaeological monitor's authority to stop and redirect grading will be exercised in consultation with the appropriate tribe in order to evaluate the significance of any archaeological resources discovered on the property. Tribal monitors shall be allowed to monitor all grading, excavation and ground breaking activities, and shall also have the authority to stop and redirect grading activities in consultation with the project archeologist.	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which requires the submittal of a pre-grading report documenting the proposed methodology for grading activity observation.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division Public Works Department - Engineering Division	Prior to approval of discretionary land use applications.
		Review and approval of the pre-grading report.	Community Development Department - Planning Division Public Works Department - Engineering Division	Prior to the issuance of grading permit.
MM Cultural/Paleontological Resources 5: The landowner shall relinquish ownership of all cultural resources, including sacred items, burial goods and all archaeological artifacts that are found on the project area to the appropriate tribe	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects'	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications and ongoing during project construction.

MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
for proper treatment and disposition.	conditions of approval which requires the landowner to relinquish ownership of all cultural resources that are found on the project area to the appropriate tribe.			
MM Cultural/Paleontological Resources 6: All sacred sites, should they be encountered within the project area, shall be avoided and preserved as the preferred mitigation, if feasible.	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which requires that all sacred sites, should they be encountered, shall be avoided and preserved, if feasible.	Compliance with project-specific conditions of approval	Community Development Department - Planning Division	Prior to approval of discretionary land use applications and ongoing during project construction.
MM Cultural/Paleontological Resources 7: If inadvertent discoveries of subsurface archaeological/cultural resources are discovered during grading, the Developer, the project archaeologist, and the appropriate tribe shall assess the significance of such resources and shall meet and confer regarding the mitigation for such resources. If the Developer and the Tribe cannot agree on the significance or the mitigation for such resources, these issues will be presented to the Community Development Director (CDD) for decision. The CDD shall make the determination based on the provisions of the California Environmental Quality Act with respect to	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which requires that if inadvertent discoveries of subsurface archaeological/cultural resources are discovered during grading, the Developer, the project	Compliance with project-specific conditions of approval	Community Development Department - Planning Division	Prior to approval of discretionary land use applications
		Review and approval of a mitigation plan agreed upon by applicant-retained qualified archaeologist and Tribal Monitor.	Community Development Department - Planning Division	Ongoing during project construction.

Section 5.0 - Mitigation Monitoring and Reporting Program



MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
archaeological resources and shall take into account the religious beliefs, customs, and practices of the appropriate tribe. Notwithstanding any other rights available under the law, the decision of the CDD shall be appealable to the City of Lake Elsinore.	archaeologist, and the appropriate tribe shall assess the significance of such resources and shall meet and confer regarding the mitigation for such resources.			
MM Cultural/Paleontological Resources 8: Individual projects implemented in accordance with the Land Use Plan shall also demonstrate compliance with Cultural and Paleontological Resources Policies 6.2 -6.4 and 7.1-7.5. As well as compliance with applicable District Plan Policies related to cultural and paleontological resources.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Cultural/Paleontological Resources 9: Individual projects implemented in accordance with the Land Use Plan shall also demonstrate compliance with Cultural and Paleontological Resources Policy 8.1. As well as compliance with applicable District Plan Policies related to cultural and paleontological resources.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Cultural/Paleontological Resources 10: If human remains are encountered, California Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the Riverside County Coroner has made the necessary findings as to origin. Further, pursuant to California Public Resources Code Section 5097.98(b) remains shall be left in place and free from disturbance until a final decision as to the treatment and disposition has been made. If the Riverside County Coroner	Project-specific CEQA environmental analysis with incorporation of a mitigation measure into individual projects' conditions of approval which addresses the accidental discovery of human remains during project construction.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.

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determines the remains to be Native American, the coroner shall contact the Native American Heritage Commission within 24 hours. Subsequently, the Native American Heritage Commission shall identify the person or persons it believes to be the "most likely descendant." The most likely descendant may then make recommendations, and engage in consultations concerning the treatment of the remains as provided in Public Resources Code 5097.98.	Applicant-retained qualified archaeologist to stop construction if human remains are encountered and to contact Riverside County Coroner.	Notification of discovery to Riverside County Coroner/Native American Heritage Commission	Community Development Department – Planning Division Qualified Archaeologist/Tribal Monitor	Ongoing during project construction.
3.3 Aesthetics				
MM Aesthetics 1: Future development projects will be required to prepare visual simulations demonstrating compliance with the applicable GPU goals and policies. Preparation of visual simulations demonstrating compliance with the GPU goals and policies would be required for future development projects located in scenic viewsheds along the I-15 corridor and other areas at the discretion of the Director of Community Development.	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
3.4 Transportation and Circulation				
MM Transportation 1: The intersection of Old Franklin Street at Auto Center Drive shall be configured as a through street parallel to I-15, with the overcrossing of the freeway forming a "T" intersection.	Review of design plans for intersection of Old Franklin Street at Auto Center Drive and freeway overcrossing.	Approval of intersection design plans.	Public Works Department – Engineering Division	At time that construction of intersection improvements is proposed.
MM Transportation 2: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate avoidance of significant impacts through implementation of the	Project-specific CEQA environmental analysis with incorporation of any mitigation	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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ultimate roadway and intersection classifications and improvements shown on the Land Use Plan and the Capital Improvement Program as well as the goals and policies set forth by the Circulation Section of the Community Form Chapter. With implementation of these goals and policies, individual projects implemented in accordance with the GPU and Land Use Plan would not result in significant and unavoidable adverse impacts on traffic levels.	measures and required roadway improvements into individual project's conditions of approval.		Public Works Department – Engineering Division	
MM Transportation 3: Individual projects implemented pursuant to the Land Use Plan within the 3rd Street Annexation will be required to demonstrate their avoidance of significant impacts through: <ul style="list-style-type: none"> implementation of the ultimate roadway and intersection classifications and improvements shown on the Land Use Plan and the Capital Improvement Program; the goals and policies set forth by the Circulation Section of the Community Form Chapter; implementation of improvements to signalization and the curve radius for the alignment from 2nd Street to Camino Del Norte identified in the Traffic Study. 	Project-specific CEQA environmental analysis with incorporation of any mitigation measures and required roadway improvements into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications.
MM Transportation 4: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate avoidance of significant impacts through implementation of the	Project-specific CEQA environmental analysis with incorporation of any mitigation	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
ultimate roadway and intersection classifications and improvements shown on the Land Use plan and the Capital Improvement Program as well as the goals and policies set forth by the Circulation Section of the Community Form Chapter.	measures and required roadway improvements into individual project's conditions of approval.		Public Works Department – Engineering Division	
MM Transportation 5: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate avoidance of significant impacts through implementation of the ultimate roadway and intersection classifications and improvements shown on the Land Use Plan and the Capital Improvement Program as well as the goals and policies set forth by the Circulation Section of the Community Form Chapter.	Project-specific CEQA environmental analysis with incorporation of any mitigation measures and required roadway improvements into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications.
3.5 Noise				
MM Noise 1: In accordance with the policies of the Lake Elsinore General Plan Update and the City's Zoning Code, the City shall require the applicant for any future development to analyze the impacts of increased traffic volume on noise conditions along affected roadways. Where project-specific analysis concludes that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the traffic noise to acceptable levels. For projects placing noise-sensitive land uses adjacent to or in the vicinity of a major roadway, the City shall require the project applicant to demonstrate the new use's compliance with City standards regarding traffic noise received on the site. Where project-specific analysis determines that noise standards may be exceeded, then the	Project-specific CEQA environmental analysis and review of discretionary land use applications during City's development review process.	Incorporation of mitigation measures into individual project's conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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City shall require binding mitigation measures that will reduce the noise received to acceptable levels. However, in some cases where realignments or upgrades of roadways are proposed or traffic levels will increase substantially like that anticipated for I-15, SR-74, Riverside Drive, Grand Avenue, Lakeshore Drive, and Lake Street there may be no mitigation that would adequately reduce future traffic noise as experienced by existing land uses or future development projects, resulting in significant and unmitigated impacts at the project level.				
MM Noise 2: For projects proposing new commercial uses in the vicinity of sensitive receptors, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.	Project-specific CEQA environmental analysis including review and approval of a project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Noise 3: For residential projects proposed adjacent to schools, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the noise received to acceptable levels. The City shall require all school projects to conduct site-specific noise analysis in accordance with State	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

MITIGATION MEASURES	IMPLEMENTATION ACTION	MONITORING METHOD	RESPONSIBLE MONITORING PARTY	TIMING
requirements.				
<p>MM Noise 4: For projects proposing new recreational uses or increased intensity of recreational activity in proximity to sensitive receptors, the City shall require the project applicant to demonstrate the recreational use's compliance with City noise standards. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.</p> <p>For projects proposing new residential uses in proximity to recreational areas, the City shall require the project applicant to demonstrate the residential use's compliance with City noise standards with respect to the existing recreational areas. Where project-specific analysis determines that noise standards may be exceeded, then the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.</p>	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
<p>MM Noise 5: For projects proposing new industrial/mining operations in the vicinity of sensitive receptors or projects that propose new sensitive uses in the vicinity of industrial/mining operations, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.</p>	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Noise 6: The City shall require 3rd Street	Project-specific CEQA	Compliance with	Community	Prior to approval of

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<p>Annexation project applicants to demonstrate their compliance with City standards regarding construction noise. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the construction noise to acceptable levels.</p> <p>For 3rd Street Annexation projects placing noise-sensitive land uses adjacent to or in the vicinity of I-15, SR-74, Cambern Avenue, and Camino del Norte, the City shall require the project applicant to demonstrate the new use's compliance with City standards regarding traffic noise received on the site. Where project-specific analysis determines that noise standards may be exceeded, then the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.</p> <p>For 3rd Street Annexation projects proposing new commercial uses in the vicinity of sensitive receptors, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines that noise standards may be exceeded, then the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.</p>	<p>environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.</p>	<p>project-specific conditions of approval.</p>	<p>Development Department - Planning Division</p>	<p>discretionary land use applications.</p>
<p>MM Noise 7: For projects that have a potential to generate construction-related groundborne vibration (e.g., use of pile drivers, rock drills, and pavement breakers), the City shall require the project applicant to submit a construction-related vibration mitigation plan to the City for review and approval. The mitigation plan shall depict the</p>	<p>Project-specific CEQA environmental analysis including review and approval of a construction-related vibration mitigation plan with incorporation</p>	<p>Compliance with project-specific conditions of approval.</p>	<p>Community Development Department - Planning Division</p>	<p>Prior to approval of discretionary land use applications.</p>

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location of the construction equipment and activities and how the vibration from this equipment and activity would be mitigated during construction of the project. The City shall require binding mitigation measures implementing the approved mitigation plan.	of any mitigation measures into individual project's conditions of approval.			
MM Noise 8: For projects proposing new industrial/mining operations in the vicinity of sensitive receptors or projects that propose new sensitive uses in the vicinity of industrial/mining operations, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines there is a potential for significant vibration-related impacts, the City shall require binding mitigation measures that will reduce the vibration received to acceptable levels.	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications.
MM Noise 9: The City shall require project applicants to demonstrate their compliance with City standards regarding construction noise. Where project-specific analysis determines that noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the construction noise to acceptable levels.	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Noise 10: For projects proposing sensitive uses that may receive airport noise, the City shall require the project applicant to demonstrate the new use's compliance with City noise standards. Where project-specific analysis determines that	Project-specific CEQA environmental analysis including review and approval of project-specific acoustical	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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noise standards may be exceeded, the City shall require binding mitigation measures that will reduce the noise received to acceptable levels.	analysis with incorporation of any mitigation measures into individual project's conditions of approval.			
3.6 Air Quality				
MM Air Quality 1: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate their avoidance of significant impacts on air quality from construction activities through implementation of regulatory requirements and the goals and policies set forth in the proposed GPU. Where project-specific analysis determines that air quality standards may be exceeded, the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable.	Project-specific CEQA environmental analysis including review and approval of project-specific air quality impact analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Air Quality 2: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate a reduction in impacts on air quality from operational emissions through implementation of goals and policies listed within the General Plan. Where project-specific analysis determines that air quality standards may be exceeded, the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable. All applicants for future development shall comply with AQMP control measures so as to reduce this impact to the greatest extent possible.	Project-specific CEQA environmental analysis including review and approval of project-specific air quality impact analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Air Quality 3: Individual projects implemented pursuant to the Land Use Plan	Project-specific CEQA environmental analysis	Compliance with project-specific	Community Development	Prior to approval of discretionary land use

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within each District Plan will be required to demonstrate a reduction in impacts on air quality from operational emissions through implementation of the General Plan's goals and policies. Where project-specific analysis determines that air quality standards may be exceeded, the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable. All applicants for future development shall comply with AQMP control measures so as to reduce this impact to the greatest extent possible.	including review and approval of project-specific air quality impact analysis with incorporation of any mitigation measures into individual project's conditions of approval.	conditions of approval.	Department – Planning Division	applications.
MM Air Quality 4: Individual projects implemented pursuant to the Land Use Plan within the 3rd Street Annexation will be required to demonstrate a reduction in impacts on air quality from operational emissions through compliance with the General Plan's goals and policies. Where project-specific analysis determines that air quality standards may be exceeded, the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable. All applicants for future development shall comply with AQMP control measures so as to reduce this impact to the greatest extent possible.	Project-specific CEQA environmental analysis including review and approval of project-specific air quality impact analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Air Quality 5: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate avoidance of significant impacts on air quality emissions associated with sensitive land uses. Where project-specific analysis determines that air quality emissions will adversely affect sensitive receptors,	Project-specific CEQA environmental analysis including review and approval of project-specific air quality impact analysis with incorporation of any	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable	mitigation measures into individual project's conditions of approval.			
MM Air Quality 6: Through the City's project review process, individual projects implemented pursuant to the Land Use Plan will be evaluated to determine their potential for creating objectionable odors that would potentially impact a substantial number of persons. Where project-specific analysis determines that objectionable odors will occur, the City shall require mitigation measures that will reduce the emissions to the greatest extent practicable.	Project-specific CEQA environmental analysis including review and approval of project-specific air quality impact analysis with incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
3.8 Biological Resources				
MM Biological Resources 1: Project-specific analysis of plant and wildlife impacts and habitat impacts completed in accordance with the MSHCP will be required to determine the significance of impacts and identify mitigation measures to reduce the impacts of future developments on plant and wildlife species and vegetation communities to less-than-significant levels.	Project-specific CEQA environmental analysis including review and approval of a project-specific analysis of plant and wildlife impacts and habitat impacts, and the incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Biological Resources 2: Project-specific analysis of habitat impacts and impacts on special-status wildlife species completed in accordance with the MSHCP and the Resource Protection and Preservation Chapter, Biological Resources Section, Goal 1, Policies 1.1-1.8 and Policy 2.2 will be	Project-specific CEQA environmental analysis including review and approval of a project-specific analysis of habitat and special-	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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required to determine the significance of impacts and identify mitigation measures to minimize the impacts to less-than-significant levels.	status wildlife species impacts, and the incorporation of any mitigation measures into individual project's conditions of approval.			
MM Biological Resources 3: Individual environmental review conducted for future development projects will be required to identify any impacts on riparian areas and wetlands and, in consultation with the appropriate resource agencies and applicable regional plans, must ensure incorporation of adequate mitigation to preserve the viability of these important biological resources.	Project-specific CEQA environmental analysis including review and approval of a project-specific analysis that identifies any impacts on riparian areas and wetlands impacts, and, in consultation with the appropriate resource agencies and applicable regional plans, incorporation of any mitigation measures into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Biological Resources 4: Not more than thirty days prior to construction activities that occur between February 1 and August 15 of any year, surveys for nesting bird species shall be conducted by a qualified biologist selected by the developer and approved by the City. If no active avian nests are identified on or within 250 feet of the limits of the construction area, up to the limits of the project	Project-specific CEQA environmental analysis with incorporation of mitigation measure MM Biological Resources 4 into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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site, no further mitigation is necessary. Alternatively, to avoid impacts, the City may allow individual projects the option of beginning construction after the previous breeding season for bird species has ended (after August 15) and before the next breeding season begins (before February 15).	Construction of individual development projects to avoid nesting season or qualified biologist to prepare surveys for nesting birds and ensure that nesting birds are avoided during construction.	Review grading permit timing and review and approval of pre-construction nesting bird surveys.	Qualified Biologist Community Development Department - Planning Division	Prior to the issuance of grading permits and ongoing during project construction.
MM Biological Resources 5: If active nests for avian species are found within the construction footprint of any future project, construction activities shall be delayed within a minimum 250-foot buffer zone surrounding nests of other special-status avian species until the young have fledged. This buffer zone shall not extend beyond the project site. No action other than avoidance shall be taken without CDFG consultation.	Project-specific CEQA environmental analysis with incorporation of mitigation measure MM Biological Resources 5 into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
	Construction of individual development projects to avoid nesting season or qualified biologist to prepare surveys for nesting birds and ensure that nesting birds are avoided during construction.	Review grading permit timing and review and approval of pre-construction nesting bird surveys.	Qualified Biologist Community Development Department - Planning Division	Prior to the issuance of grading permits and ongoing during project construction.
3.9 Hydrology and Water Quality				
MM Hydrology 1: The following goals and policies of the GPU must be implemented as a part of future development to mitigate potential	Review of discretionary land use applications during City's	Determination of project consistency with General Plan and	Community Development Department - Planning	Prior to approval of discretionary land use

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<p>impacts associated with 1) alteration of drainage patterns and associated erosion; 2) development within the 100 year floodplain and 3) water quality:</p> <ul style="list-style-type: none"> Flooding and Floodplains Policies 5.1–5.2 Water Resources Policies 4.1–4.4 Biological Resources Policies 1.1–1.8 and 2.1–2.2 	development review process, including project-specific CEQA environmental analysis and the incorporation of any appropriate mitigation measures into individual project's conditions of approval.	compliance with project-specific conditions of approval.	<p>Division</p> <p>Public Works Department – Engineering Division</p>	applications.
3.10 Hazards and Hazardous Materials				
MM Hazards 1: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate their avoidance of significant impacts associated with use and storage of hazardous materials and disposal of hazardous waste through implementation of Policies 3.1 through 3.4 of the Hazards and Hazardous Materials section of the Public Safety and Welfare chapter.	Review of discretionary land use applications during City's development review process, including project-specific CEQA environmental analysis and the incorporation of any appropriate mitigation measures into individual project's conditions of approval.	Determination of project consistency with General Plan and compliance with project-specific conditions of approval.	<p>Community Development Department – Planning Division</p> <p>Public Works Department – Engineering Division</p>	Prior to approval of discretionary land use applications.
MM Hazards 2: Individual projects implemented pursuant to the Land Use Plan within the District Plans will be required to demonstrate their avoidance of significant impacts associated with exposure to hazardous materials through implementation of Policy 3.5 of the Hazards and Hazardous Materials section of the Public Safety and Welfare chapter. Proposed development projects on or adjacent to the SARI line in these districts would be required to analyze risks specific	Review of discretionary land use applications during City's development review process, including project-specific CEQA environmental analysis and the incorporation of any appropriate mitigation measures	Determination of project consistency with General Plan and compliance with project-specific conditions of approval.	<p>Community Development Department – Planning Division</p> <p>Public Works Department – Engineering Division</p>	Prior to approval of discretionary land use applications.

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to sensitive land uses and the extent of the subsurface components involved with building in these locations.	into individual project's conditions of approval.			
MM Hazards 3: Individual projects implemented pursuant to the Land Use Plan within the 3rd Street Annexation will be required to demonstrate their avoidance of significant impacts associated with use and storage of hazardous materials and disposal of hazardous waste through implementation of Policies 3.1 through 3.4 of the Hazards and Hazardous Materials section of the Public Safety and Welfare chapter.	Review of discretionary land use applications during City's development review process, including project-specific CEQA environmental analysis and the incorporation of any appropriate mitigation measures into individual project's conditions of approval.	Determination of project consistency with General Plan and compliance with project-specific conditions of approval.	Community Development Department – Planning Division Public Works Department – Engineering Division	Prior to approval of discretionary land use applications.
MM Hazards 4: Proposed development projects within the Skylark Airport Influence Area, as shown on Figure 2.7 of the General Plan, will be evaluated for consistency with continued operations at the airport. The project applicant of each such development project shall comply with the applicable requirements of the Federal Aviation Administration (FAA) regarding any encroachment into the airport's navigable airspace in accordance with Federal Aviation Regulations (FAR) Part 77.	Review of discretionary land use applications within proximity to the Skylark Airport during City's development review process, including project-specific CEQA environmental analysis; and where appropriate, the incorporation of the requirement to comply with FAR Part 77 requirements into individual project's conditions of approval.	Compliance with project-specific conditions of approval.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
MM Hazards 5: Individual projects implemented	Review of discretionary	Determination of	Community	Prior to approval of

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pursuant to the Land Use Plan in each District and within the 3rd Street Annexation Area will be required to demonstrate their avoidance of significant impacts associated with wildfire hazards through implementation of all policies under the Wildfire Hazards section of the Public Safety and Welfare chapter.	land use applications during City's development review process, including project-specific CEQA environmental analysis and the incorporation of any appropriate mitigation measures into individual project's conditions of approval.	project consistency with General Plan and compliance with project-specific conditions of approval.	Development Department - Planning Division City of Lake Elsinore Fire Department	discretionary land use applications.
3.11 Geology and Soils				
MM Geology and Soils 1: Individual projects implemented pursuant to the proposed project will be required to demonstrate their avoidance of significant impacts associated with seismic hazards including ground-shaking, liquefaction, landslides, subsidence and collapse through implementation of all goals and policies under the Land Use section of the Community Form Chapter and the Seismic Activity section of the Public Safety and Welfare chapter of the GPU.	Project-specific CEQA environmental analysis including review and approval of project-specific geotechnical investigation with incorporation of any mitigation measures into individual project's conditions of approval.	Review and approval of project-specific geotechnical investigation.	Public Works Department - Engineering Division Community Development Department - Planning Division	Prior to approval of discretionary land use applications.
MM Geology and Soils 2: The City shall continue to enforce the seismic design provisions for Seismic Zone 4 of the California Building Code, including near-source seismic conditions for all new construction in the City.	Project-specific CEQA environmental analysis including review and approval of project-specific geotechnical investigation with incorporation of any mitigation measures into individual project's	Review and approval of project-specific geotechnical investigation.	Public Works Department - Engineering Division Community Development Department - Planning Division	Prior to approval of discretionary land use applications.

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	conditions of approval.			
MM Geology and Soils 3: Individual projects implemented pursuant to the proposed project will be required to demonstrate their avoidance of significant impacts associated with expansive or corrosive soils through implementation of the policies under the Seismic Activity section of the Public Safety and Welfare chapter.	Project-specific CEQA environmental analysis including review and approval of project-specific geotechnical investigation with incorporation of any mitigation measures into individual project's conditions of approval.	Review and approval of project-specific geotechnical investigation.	Public Works Department – Engineering Division Community Development Department – Planning Division	Prior to approval of discretionary land use applications.
Section 3.14 Public Services				
MM Public Services 1: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate their avoidance of significant impacts associated with public services related to 1) police service, 2) fire protection, 3) schools, 4) libraries, , and 5) animal control through implementation of the following: <ul style="list-style-type: none"> • Compliance with applicable State and local laws and regulations, • Policy 1.6 of the Community Form chapter, Land Use section, • Policies 8.1 through 8.4 under Goal 8 of the Community Facilities and Protection Services section of the Public Safety and Welfare chapter, and • Goals 9 through 11 and associated policies of the Community Facilities and Protection Services section of the Public 	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department – Planning Division	Prior to approval of discretionary land use applications.

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Safety and Welfare chapter addressing schools, libraries, and animal control services.				
Section 3.15 Parks and Recreation				
MM Parks and Recreation 1: Individual projects implemented pursuant to the Land Use Plan will be required to demonstrate their avoidance of significant impacts associated with community services related to parks and recreation through implementation of the following: <ul style="list-style-type: none"> • Policies under Goals 8 and 9 of the Parks and Recreation section of the Community Form chapter. • Policies 1.1 and 2.1 of the Community Form chapter, Land Use section. 	Review of discretionary land use applications during City's development review process.	Determination of project consistency with General Plan.	Community Development Department - Planning Division	Prior to approval of discretionary land use applications.